Report to the Future Melbourne (Planning) Committee

Agenda item 6.2

City of Melbourne submission regarding Planning Scheme Amendment C281: Games Village Project Parkville

2 May 2017

Presenter: Emma Appleton, Manager Urban Strategy

Purpose and background

1. The purpose of this report is to seek the Future Melbourne Committee's endorsement of a Melbourne City Council submission regarding Amendment C281 (the Amendment) for the Commonwealth Games Village site (the Site). The Amendment was prepared by the Department of Environment, Land, Water and Planning (DELWP) as the Minister for Planning is the Responsible Authority for the Site as specified under the schedule to Clause 61.01 of the Melbourne Planning Scheme.

2. The Amendment is sought by the landowners, Village Park Consortium, together with Major Projects Victoria.

3. The Site is located in Parkville and generally bounded by Oak Street to the east, Park Street to the north, Citylink Freeway to the west and Royal Park (including the Trin Warren Tam-boore wetlands, which provides some of the water used for Royal Park) to the south (see Attachment 2).

4. Development of the Site is currently subject to an Incorporated Document 'The Games Village Project, Parkville' (September 2006), which includes a masterplan for the Site.

5. To date, stages 1-3 have been constructed (2-4 storey buildings). Stage 4 has been approved (11 habitable storeys), and Stages 5-7 are underway (8-11 habitable storeys). Stage 8 has been approved at 13 habitable storeys (see Attachment 3). Stage 12 has been approved at 10 habitable storeys (with an additional application for the same stage lodged with DELWP proposing further changes to setbacks). Amendment C281 affects Stages 9-12.

6. The Amendment seeks to permit an increase in height for the remaining stages of the Freeway Apartments Precinct (Stages 9-12, see Attachment 3). The following changes to building heights are proposed:

    6.1. Stage 9: approximately 67m (increased from approximately 40m)

    6.2. Stage 10: approximately 61m (increased from approximately 43m)

    6.3. Stage 11: approximately 79m (increased from approximately 40m)

    6.4. Stage 12 (already approved): approximately 54m (increased from approximately 49m).

7. The Amendment is currently on exhibition until 5 May 2017 and is available online. Residents within the area were notified by DELWP (Attachment 4) and may make a submission.

Key issues

8. In 2016 prior to exhibition, officers were invited by DELWP to review the proposed new plans for the Site. Officers advised DELWP of their concern regarding the impacts of the increased heights. No changes were made in response to officers' comments.

9. Officers have prepared a submission to Amendment C281 highlighting that the proposed changes are not supported for the following reasons:

    9.1. The proposal does not have sound planning justification, as it does not respond appropriately to the context or envisaged outcome as a cohesive mid-rise precinct, or and demonstrate any clear community benefit.

    9.2. The proposed building heights and mass (Stages 9-11) will increase the visual bulk and dominance of the building in long views, and impact on the visual amenity of the adjoining linear park, Trin Warren Tam-boore wetlands and Royal Park.

    9.3. The proposed siting and setbacks of the towers do not provide for the level of amenity sought in the existing controls, impacting on outlook, overlooking and access to daylight.

    9.4. The proposed depth of the tower footprints (altering the aspect of apartments) would impact negatively on the daylight amenity of the apartments.

Recommendation from management

10. The Future Melbourne Committee:

    10.1. Determines to send a letter to the Minister for Planning advising that the Melbourne City Council does not support proposed Amendment C281 for the reasons outlines in the submission (attachment 5).

    10.2. Requests that if any increase in development is eventually authorised, that this increase should be subject to development contributions to provide improved services for resultant increase in population.

Attachments:

Attachment 1
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Supporting Attachment

Legal

1. The proposed Amendment is the responsibility of DELWP. The Council is entitled to make a submission.

Finance

2. There is no cost to the Council.

Conflict of interest

3. No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a direct or indirect interest in relation to the matter of the report.

Stakeholder consultation

4. It is the responsibility of DELWP to consult with key stakeholders.

5. The Council has a web page with a summary of the amendment and an explanation that the Minister for Planning is the responsible authority for this amendment. Submissions are directed to the Minister for Planning.

Relation to Council policy

6. The Melbourne Planning Scheme encourages high quality urban design outcomes that respond to its context, as stated in Clauses 11, 15, 21.03, 21.04, 21.06 and 21.07.

Environmental sustainability

7. The proposed amendment is the responsibility of DELWP.

Attachments:
1. Supporting attachment
2. Site location
3. Plan showing the existing controls compared to the proposed controls
4. Area of notification
5. Draft submission to DELWP
6. Amendment C281 amendment documents

Attachment 2: Site location and photos

Broader Games Village site

Subject site 'freeway apartments' within the broader Games Village Site

Attachment 2: Site location and photos




Trin Warren Tam-boore Wetlands (top) and Galada Avenue Reserve (bottom)


Other development within the Village, including within Stage 1 of the redevelopment (top) and heritage protected buildings on Cade Way (bottom)


Development constructed in earlier stages of the Games Village Redevelopment in line with existing controls

Attachment 3
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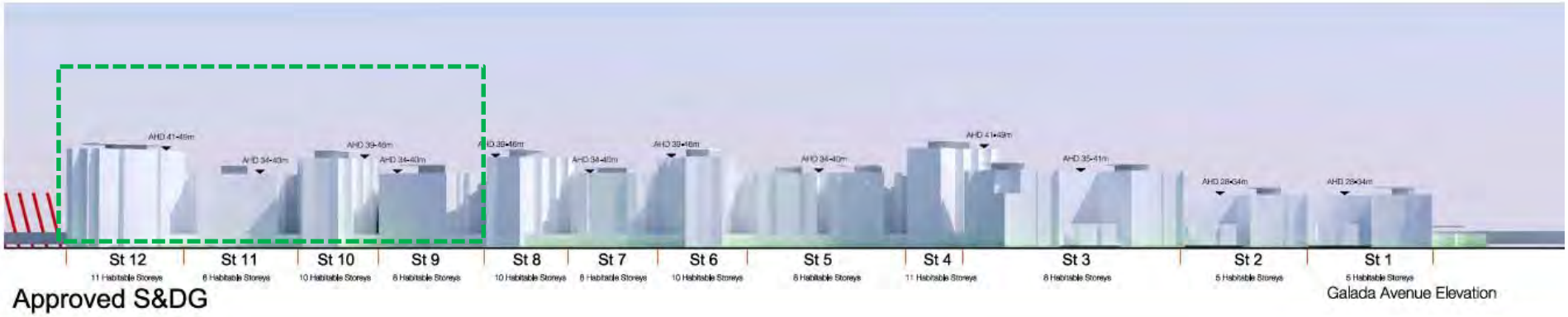
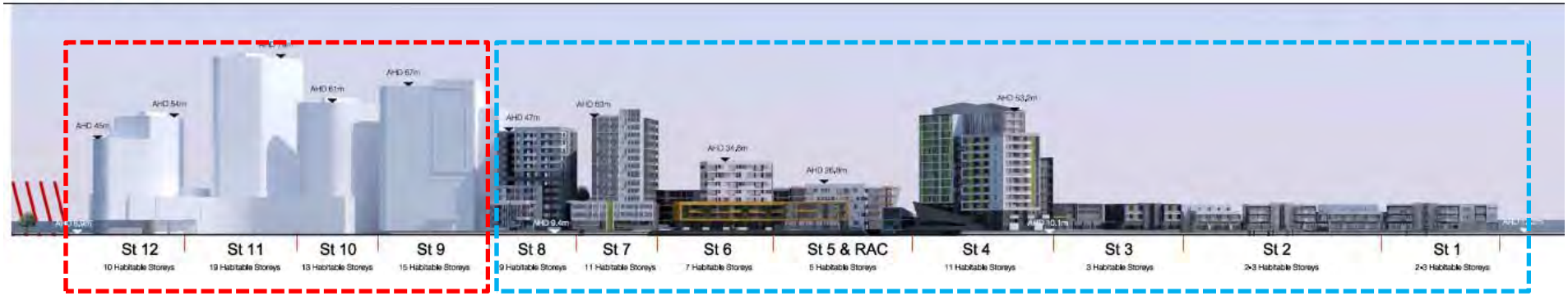
Attachment 3: siting and design guidelines showing permitted built form under existing (top) and proposed controls (bottom), and constructed (or approved) development (bottom right)

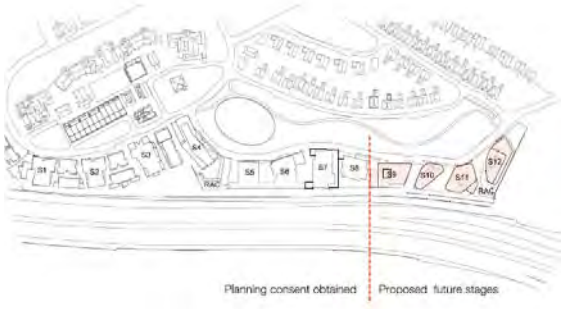




Stages 1-9 (already built or approved)

Stages 9-12 (built form permitted under existing controls)

Stages 9-12 – proposed development (subject of C281)



Attachment 4: Area of notification for C281

Boundary for area of notification

SUBMISSION FOR AMENDMENT C281: GAMES VILLAGE PROJECT, PARKVILLE

The City of Melbourne welcomes the opportunity to make a submission regarding Amendment C281: Games Village Project, Parkville.

1. History

The site was partially developed by the State Government, as the landowner, in 2005 and 2006 to accommodate athletes during the Melbourne 2006 Commonwealth Games. Subsequent stages of the project envisaged further residential development.

In 2006, an Incorporated Document was brought into the Melbourne Planning Scheme 'The Games Village Project, Parkville 17 October 2006'. This envisaged 900 dwellings comprising a modulated line of freeway apartments on the western boundary. To date, stages 1-3 have been constructed (2-4 storey buildings). Stage 4 has been approved (11 habitable storeys), and Stages 5-7 are underway (8-11 habitable storeys). Stage 8 has been approved at 13 habitable storeys. Stage 12 has been approved at 10 habitable storeys (with an additional application for the same stage lodged with DELWP proposing further changes to setbacks). Some of these stages included variations from the existing controls, but each was assessed individually. Amendment C281 affects Stages 9-12.

2. Site context

The site forms part of the former Commonwealth Games Village site in Parkville. It is adjacent to the freeway noise wall to the west, Royal Park to the south, low rise housing to the east and low rise housing to the north east. It includes heritage protected period buildings to the north east. The development of the Games Village site as a whole is to accommodate residential development, aged care, and a small mixed use precinct with a mix of residential, retail, cafés and commercial uses.

The 'freeway apartments' part of the site, adjacent to the freeway noise wall, is the subject of this amendment. It comprises the western-most precinct of the Games Village, and is adjacent to linear public park to the east 'Galada Avenue Reserve', and Royal Park's Trin Warren Tam-boore Wetlands to the south. The Wetlands are of ecological and heritage significance, and provide a significant amount of the water for irrigation of Royal Park.

Figure 1 showing photos of the site and parkland including looking west towards the site from Galada Avenue Reserve (top left), the already constructed Stage 8 of the development (top right), Galada Avenue Reserve (bottom left) and Trim Warren Tam-boore Wetlands (bottom right)

3. Plan Melbourne and the Municipal Strategic Statement (MSS)

Plan Melbourne is the State Government's strategic document (updated in March 2017) to guide the growth of Melbourne to 2050. Plan Melbourne directs new housing to urban renewal precincts, and it is also supported in activity centres and other places that offer good access to jobs, services and public transport.

The Games Village site is not within an activity centre, or within an identified urban renewal precinct. It is also not within or adjacent to the Parkville National Employment and Innovation Cluster (NEIC) identified in Plan Melbourne – referring to the cluster of education, research and medical facilities along Royal Parade and surrounding area. The City of Melbourne Growth Area Framework Plan (Clause 21.04) in the MSS also does not identify the site as an urban renewal area, or proposed or potential urban renewal area. This demonstrates that there is no strategic justification for the level of intensive residential development proposed by Amendment C281in either Plan Melbourne or the City of Melbourne's MSS.

However, development in the form of medium and higher density development is supported in Plan Melbourne where a site has proximity to public transport, an activity centre or a NEIC. On the basis of the site's proximity to public transport, the opportunity for higher density development is acknowledged, but it is argued that this has already been enabled by current controls which result in a mid-rise development. The increase in height and yield proposed in Amendment C281 would exceed expectations of many of the urban renewal areas or activity centres and is therefore deemed as overdevelopment of the site.

4. Current Planning Controls

The site is within the Residential Growth Zone (RGZ), where increased densities are envisaged up to four storeys. Specific planning controls that apply to the site are contained within the Melbourne Planning Scheme Incorporated Document 'The Games Village Project, Parkville 17 October 2006'. This includes a control specifying that a masterplan that provides for at least 900 dwellings comprising a modulated line of freeway apartments on the western boundary not exceeding 11 habitable room storeys in height must be prepared. The 900 dwellings are to be provided in 12 stages.

5. The Proposal

Planning Scheme Amendment C281 proposes changes to the controls that guide the built form and development on this site in order to permit greater heights for the remaining stages of the 'freeway apartments' component of the development.

The City of Melbourne submits that the proposed changes to the controls guiding development of the Games Village Project in Parkville will enable development which does not align with the vision for the area. Instead, the proposed changes will result in a bulky built form that affects the amenity of Royal Park and the adjacent Galada Avenue Reserve, and dominates key vantage points throughout the area. Proposed changes will also affect the internal amenity for residents. In addition, it has not been demonstrated how the increased population will be supported by necessary services.

5.1 Planning justification and deviation from current controls

The proposed development does not provide sufficient justification to vary from the existing controls guiding development on the site.

More intensive development in this area is not supported in either Plan Melbourne or the MSS (as stated previously).

Stages 1, 2, 3, 5 and 6 are lower in height than the maximum heights permitted under the existing controls. However, this is not a planning rationale for greater height for the remaining buildings. This approach to transfer of yield and increase of building height fails to consider the context, including the impact to the character of the neighbourhood, significant parks or view corridors. The increasing use of this approach by developers is concerning and departures from the approved plans should be based on a sound planning justification, with demonstrated consideration of context and the merit of each building.

The proposal does not provide planning justification to vary from the existing controls. References to public benefit through provision of affordable housing do not include a formal commitment, and therefore do not support a rationale for increased density or height.

The proposed changes include building heights of almost 80m (almost double the maximum heights expected under current provisions). An 80m height is similar to the expectation for built form in parts of the extended central city. It is higher than development in proposed urban renewal areas, such as City North or Arden Macaulay, which are part of a mixed use area with a retail and commercial hub, close to services and a range of public transport options.

This part of Parkville does not have the same strategic assets. The context is different. Therefore, a different outcome is envisaged of a cohesive mid-rise precinct which would have been enabled by the current controls that apply to the site.

5.2 Increased population and access to services

The proposal includes a total yield of 1,288 dwellings, an increase of 388 dwellings (more than 40%) above the 900 dwellings envisaged in existing controls. Existing controls specify that the site should accommodate at least 900 dwellings, so it is acknowledged that there is some flexibility to exceed this. However, an additional 388 dwellings could result in more than 750 people additional people (based on the average household size in the City of Melbourne of 1.95 people per dwelling). This would bring the total population to 2,511, which has implications in terms of access to services and infrastructure.

Although the site is close to public transport, located approximately 200m from the nearest train station and closer to bus route 505, it is not well serviced by shops and essential services. The closest shopping areas are Mt Alexander Road, Travancore, and Racecourse Road, Flemington, which are both approximately 2km away from the site on the western side of Citylink. Schools and community centres are also between 1.5 and 2km from the site.

The needs of the existing and future population should be considered as critical to any proposal with adequate services provided to support communities. The proposal does not include a mix of uses.

6. Built form considerations

The City of Melbourne has a number of concerns with the built form of the proposed development, relating to its response to the context, overshadowing of public space, design quality and internal amenity.

6.1 Context

Importance as a city gateway

The site is located to the east of Citylink, close to the International Gateway Sculptures. The existing development within the Village is highly visible from the freeway and further development of similar and higher scale will impact on an important gateway to the city.

The freeway edge is treated as a non-sensitive interface by the proposal, but buildings are highly visible from the freeway and dominate the view at this important gateway to and from central Melbourne. Figure 2 below shows the proposed development from Citylink. The proposed development presents an unreasonably bulky form, with backs of buildings to Citylink. Any development along this edge should consider this aspect and view. Where built form is visible from this gateway, they should demonstrate architectural excellence as they will impact on visitors' impressions of Melbourne.

Figure 2 showing proposed development from Citylink outbound (top) and inbound (bottom)

Public space

The proposed development which would be delivered by Amendment C281 will overshadow Galada Avenue Reserve to the east of the buildings to an unreasonable extent, with the increase in height and bulk detrimentally impacting the amenity of the park.

Whilst shadow diagrams indicate that there will not be a substantial increase in overshadowing of the Trin Warren Tam-boore Wetlands, Galada Avenue Reserve to the east of the buildings will largely be in shadow in the afternoons. The additional height proposed in stages 9-12 of the proposed development exacerbates the shadowing of this area.

Figure 3 shows minimal additional shadow to Trin Warren Tam-boore wetlands although we note that these wetlands are critical to the management of the Royal Park ecosystem and irrigation network (note that shadow diagrams have not been provided showing the impact on Galada Avenue Reserve)

The interface of the proposed development and the parkland, both the Galada Avenue Reserve and the Trin Warren Tam-boore wetlands, is also affected by the proposal. Although there is a modest increase in height proposed for Stage 12, the increased height sought for stages 9, 10 and 11 will result in greater visual bulk when viewed from the parklands. Figures 3 and 4 demonstrate the visibility and impact of the proposed development on the parks.

Figure 3 showing proposed development and its relationship with Galada Avenue Reserve and Trin Warren Tam-boore wetlands

Figure 4 showing the proposed development and its visibility from Royal Park

Clause 10.1 of the existing (and proposed) controls seeks "*to integrate the Games Village project with surrounding communities and parkland".* As shown in Figure 4, this objective is not achieved by the proposed buildings as the bulk of the proposed development will detrimentally impact on these spaces.

6.2 Visual bulk, massing and internal amenity

The visual bulk of the proposal is exacerbated by the proposed depth and lack of separation between towers, and proposed increase in height.

The proposed siting of the buildings and their setbacks are considered to be ineffective at providing for the level of amenity sought in the design objectives of the existing controls. The building depth and orientation of the proposed development (particularly stages 10 and 11) alters the aspect of the apartments from being predominantly east-west with outlook toward the parklands, to north-south, affecting residents' outlook and access to daylight.

6.3 Design quality

Clause 10.3 of the controls seeks "t*o promote high quality apartment developments that make a positive contribution to the built form of the area"*. It is essential that the controls facilitate high quality architecture, given the buildings high visibility from CityLink, and from the surrounding area. Applications for development should be referred to the OVGA's Victorian Design Review Panel at key stages of the design process, to ensure high quality design. They will set precedent for future development along this corridor.

7. Recommendations

The changes to the controls in proposed Amendment C281 are not supported. The following changes to the proposal are recommended:

a) Reduce the heights, mass and bulk of stages 9-11 to limit the visual impact of the development on the surrounding area, and improve the amenity of Galada Avenue Reserve and Trin Warren Tam-boore wetlands and Royal Park
b) Alter the depth and siting (including tower separation) of the buildings to ensure reasonable internal amenity and outlook for residents
c) Ensure focus on high quality architecture at each interface of the buildings, by including iterative reviews of the proposed development by the Victorian Design Review Panel at key stages of the design process
d) Providing a formal commitment to the inclusion of affordable housing in the proposal
e) Ensuring commensurate services are provided to support the increase in population on the site
f) Ensuring if any increase in development is eventually authorised, that this increase should be subject to development contributions to provide improved services for resultant increase in population.

Attachment 6: Amendment C281 amendment documents and supporting documents

Planning and Environment Act 1987

MELBOURNE PLANNING SCHEME

AMENDMENT C281

INSTRUCTION SHEET

The planning authority for this amendment is the Minister for Planning.

The Melbourne Planning Scheme is amended as follows:

Planning Scheme Ordinance

The Planning Scheme Ordinance is amended as follows:

- In Particular Provisions – Clause 52.03, replace the Schedule with a new schedule in the form of the attached document.
- In Incorporated Documents – Clause 81.01, replace the Schedule with a new Schedule in the form of the attached document.

End of document

Planning and Environment Act 1987

MELBOURNE PLANNING SCHEME

AMENDMENT C281

EXPLANATORY REPORT

Who is the planning authority?

This amendment has been prepared by the Minister for Planning, who is the planning authority for this amendment.

The Amendment has been made at the request of Village Park Consortium.

Land affected by the Amendment

The Amendment applies to:

The Games Village Project, Parkville, is generally bound by Oak Street to the east, Park Street to the north, the Citylink Freeway to the west and Royal Park to the south and is identified on the below map:

What the amendment does

The Amendment proposes to amend the schedules to clauses 52.03 and 81.01 to insert an Incorporated Document titled "The Games Village Project, Parkville, September 2015".

Strategic assessment of the Amendment

Why is the Amendment required?

The amendment is required to facilitate updating the "Village Park Commonwealth Games Village 2006 Master Plan 07 September 2015" (Master Plan) and the "Parkville Gardens Freeway Apartments Siting and Design Guidelines, Amended June 1, 2016" (S&D Guidelines).

Specifically the amendment facilitates an altered urban design outcome for the remaining stages of the Freeway Apartments Precinct (Stages 9 – 12), including an increase in habitable storeys to some buildings, but does not increase the average habitable storeys of the apartment precinct. Such an outcome would otherwise be prohibited under the current planning scheme controls which apply to the site.

How does the Amendment implement the objectives of planning in Victoria?

The Amendment is consistent with the objectives of Planning in Victoria as it will provide for the fair, orderly, economic and sustainable use and development of the land. Specifically it will implement the following objectives of planning in Victoria:

- Securing a pleasant, efficient and safe working, living and recreational environment.
- Facilitating development in accordance with a number of objectives.
- Balancing the present and future interests of all Victorians.

How does the Amendment address any environmental, social and economic effects?

The Amendment addresses environmental effects by facilitating future residential development that is compatible with the surrounding residential area and context. Stage 12 which is located closer to the existing established residential area and Royal Park Wetlands to the south, will have a reduction in Habitable Storeys and the Amendment will not cause any additional overshadowing onto the wetland above the already endorsed extent. The Amendment will also facilitate the delivery of a larger public open space that connects the wetlands to the south to the existing chain of parks located within the Games Village estate.

The proposed amendment will support a more socially and economically sustainable development for Stages 9-12 of the Freeway Apartments Precinct by concentrating residential development in a well serviced area. The Amendment will assist in the delivery of additional social housing and housing choice in the Parkville area.

Does the Amendment address relevant bushfire risk?

The site is not located in a designated bushfire prone area and does not present a bushfire risk.

Does the Amendment comply with the requirements of any Minister's Direction applicable to the amendment?

The amendment complies with the requirements of the Ministerial Direction on the Form and Content of Planning Schemes pursuant to section 7(5) of the Act.

Pursuant to section 12 of the Act the amendment complies with the following applicable Ministerial Directions:

- Ministerial Direction 9: Metropolitan Strategy;

The amendment is consistent with *Plan Melbourne, Metropolitan Planning Strategy*, by facilitating significant housing opportunities with an area identified for growth and providing increased housing choice and affordability.

- Ministerial Direction 11: Strategic Assessment of Amendments;

  This direction seeks to ensure a comprehensive strategic evaluation of a planning scheme amendment. This Explanatory Report addresses the requirements outlined in this direction.

How does the Amendment support or implement the State Planning Policy Framework and any adopted State policy?

The amendment supports the State Planning Policy Framework by providing an improved Incorporated Document that will facilitate changes to the Master Plan and S&D Guidelines for a large scale urban renewal site close to Melbourne's CBD, which will support housing choices and affordability and jobs.

Specifically the amendment is consistent with Clause 11.04-1 Delivering jobs and investment, Clause 11.04-2 Housing choice and affordability, Clause 11.04-3 A more connected Melbourne, Clause 11.04-4 Liveable communities and neighbourhoods, Clause 11.04-5 Environment and water and Clause 16 Housing.

How does the Amendment support or implement the Local Planning Policy Framework, and specifically the Municipal Strategic Statement?

The amendment supports the Local Planning Policy Framework by maintaining parkland values and residential amenity. The Amendment is consistent with the preferred settlement pattern for Melbourne, accommodating growth beyond Central City (Hoddle Grid, Docklands and Southbank) in strategically justified locations (Clause 21.03).

The amendment is consistent with the vision for the area which seeks to accommodate more intensive residential in-fill development in the 'Parkville Gardens Estate' (Clause 21.16-4). The Amendment also supports the provision of affordable, safe and well designed and managed student housing in locations with good access to public transport, services and tertiary education facilities (Clause 21.07-1).

Does the Amendment make proper use of the Victoria Planning Provisions?

It is considered that the proposed Amendment makes proper use of the Victoria Planning Provisions. The land is already subject to a site specific exemption. Amending the existing Incorporated Document is considered to be the proper mechanism to facilitate an improved urban design outcome that is more responsive to the surrounding context and accommodate increased housing choice in a well serviced area.

How does the Amendment address the views of any relevant agency?

The Amendment complies with the relevant requirements of the Transport Integration Act, specifically Part 2, Division 2, 11 – Integration of transport and land use.

Resource and administrative costs

What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?

The amendment is unlikely to have an adverse impact on resource and administrative costs to the responsible authority.

Where you may inspect this Amendment

The amendment is available for public inspection, free of charge, during office hours at the following places:

- at the Department of Environment, Land, Water and Planning website www.delwp.vic.gov.au/public-inspection

- at the office of the Department of Environment, Land, Water and Planning, 8 Nicholson Street, East Melbourne **(by appointment only, please call 8392 5505),** and,

- at the office of the City of Melbourne, Council House 2, Level 3, 240 Little Collins Street, Melbourne.

The amendment can also be inspected free of charge at the Department of Environment, Land, Water and Planning website at www.delwp.vic.gov.au/public-inspection

Submissions

Any person who may be affected by the amendment may make a submission to the planning authority. Submissions about the amendment must be received by **5pm Friday 5 May 2017**.

A submission must be sent to: The Minister for Planning, c/- The Department of Environment, Land, Water PO Box 500, MELBOURNE VIC 3001, quoting Amendment Melbourne C281.

Alternatively, a submission can be made online at www.planning.vic.gov.au/melbourne-c281

Panel hearing dates

In accordance with clause 4(2) of Ministerial Direction No.15 the following panel hearing dates have been set for this amendment:

- directions hearing: week commencing **26 June 2017**

- panel hearing: week commencing **17 July 2017**

02/3/2017
~~C310~~
Proposed C281

SCHEDULE TO CLAUSE 52.03

Address of land	Title of incorporated document
Kensington Banks Redevelopment Site, Kensington	Kensington Banks Development Plan (Subdivisions)
42 Clarendon Street, South Melbourne	Sky sign - 42 Clarendon Street, South Melbourne
766 Elizabeth Street, Carlton	High wall signs - 766 Elizabeth Street, Carlton
Former Queen Victoria Hospital Site, Melbourne	Former Queen Victoria Hospital Site, Open Lot Car Park, Melbourne
346-376 Queen Street, 334-346 La Trobe Street and 142-171 A'Beckett Street	346-376 Queen Street, 334-346 La Trobe Street and 142-171 A'Beckett Street Open Lot Car Park, Melbourne
218 Berkeley Street, 243, 249, 251 and 253 Grattan Street, Vol 9586 Fol 585 153 Barry Street, Vol 8277 Fol 993 155 Barry Street, Vol 8277 Fol 994 157 Barry Street, Vol 8733 Fol 185 159 Barry Street, Vol 8252 Fol 839 161-163 Barry Street, Vol 8273 Fol 464 95 Barry Street, Vol 8651 Fol 154 97 Barry Street, Vol 4454 Fol 890686 99 Barry Street, Vol 9310 Fol 338 101 Barry Street, Vol 990 Fol 598 103-105 Barry Street, Vol 2659 Fol 700 107 Barry Street, Vol 9605 Fol 235 109 Barry Street, Vol 9605 Fol 236 111-117 Barry Street, Vol 9968 Fol 851, Vol 4744 Fol 948694 and Vol 6549 Fol 1309658 119-129 Barry Street, Vol 10042 Fol 777 Part (combined with 131-137 Barry Street and 200 Berkeley) 179-187 Pelham Street, Vol 8149 Fol 784 Leicester Street, Vol 4164 Fol 832756 149 Leicester Street, Vol 8369 Fol 589 and Vol 8369 Fol 320 62 Barry Street, Vol 8041 Fol 082 239-241 Bouverie Street, Vol 9955 Fol 707 Victorian Bowling Club, Grattan Street, Part Vol 0600 Fol 912 216-222 Leicester Street, Graduate Union, subterranean rights for carpark access, Vol 9767 Fol 292 202-216 Pelham Street also identified as 162-178 Berkeley Street, Vol 8986 Fol 351, Vol 9254 Fol 506, Vol 4152 Fol 341, Vol 1167 Fol 332, Vol 8986 Fol 350, Vol 8965 Fol 641,	University of Melbourne, University Square Campus, Carlton, November 1999

Address of land	Title of incorporated document
Vol 8390 Fol 463, Vol 8965 Fol 640	
Spencer Street Station redevelopment precinct generally bounded by Spencer Street, Collins Street, Wurundjeri Way and La Trobe Street Melbourne, (excluding the Melbourne City Mail Centre)	Spencer Street Station redevelopment, June 2013
Crown Allotment 21D, Power Street, Southbank	Promotional Panel sign, Crown Allotment 21D, Power Street, Southbank, July 1999
29 Simpson Street, East Melbourne	Cliveden Hill Private Hospital, 29 Simpson Street, East Melbourne, July 1999
236-254 St Kilda Road, Southbank	Mirvac, Residential Towers, 236-254 St Kilda Road, Southbank
95-129 Bourke Street, 113-149 Exhibition Street, and 78-120 Little Collins Street, Melbourne	Former Southern Cross Hotel site, Melbourne, March 2002
172-192 Flinders Street and 189-195 Flinders Lane, Melbourne	Flinders Gate car park, Melbourne, July 1999
Land comprising public lands and sports and entertainment facilities within the area bounded generally by Flinders Street, Wellington Parade, Punt Road, South-Eastern Freeway, Batman Avenue and Exhibition Street Extension, Melbourne	Sports and Entertainment Precinct, Melbourne, August 2007
1 Swanston Street, Melbourne	Young and Jackson's Hotel, Promotional Panel Sky sign, Melbourne, July 1999
Land comprising public lands and sports facilities for the State Netball and Hockey Centre, Brens Drive Royal Park, Parkville	State Netball and Hockey Centre, Brens Drive Royal Park, Parkville, May 2000
St Kilda Road adjacent to Princes Bridge Melbourne; Sports and Entertainment Precinct, (Batman Avenue and Swan Street), Melbourne	Federation Arch and Sports and Entertainment Precinct Signs, April 2002
Road reservations of Victoria Parade, East Melbourne, Gisborne Street, Macarthur Street, Collins Street and Spencer Street, Melbourne, and Clarendon Street and Normanby Road, South Melbourne	Tram Route 109 Disability Discrimination Act compliant Platform Tram Stops, August 2007
Melbourne Aquarium, Enterprise Park/Batman Park	Melbourne Aquarium Signs, July 2001
2-26 and 30-50 Southbank Boulevard, 17-23 Queensbridge Street and 127-131 and 133-141 Queensbridge Square, Southbank	Freshwater Place, Southbank, August 2001 (Amended 2012)
4 Lloyd Street, Kensington	Simplot Australia head office, Kensington, October 2001
Bio 21 Project area: land bounded by Flemington Road, Park Drive, Story Street, Parkville and abutting University High School and Royal Melbourne Hospital to the east, Title Vol 10565 Fol 172	University of Melbourne Bio21 Project Parkville, July 2015
349-373 Swanston Street, 183-265 and 214-252 La Trobe Street, 316-364 Elizabeth Street, 198-262 and 285-307 Little Lonsdale Street and 284-310 Lonsdale Street, Melbourne	Melbourne Central redevelopment March 2002

Address of land	Title of incorporated document
46-74 Flinders Street, Melbourne	Former Herald and Weekly Times Building, 46-74 Flinders Street, Melbourne, August 2002
South Tower -Rialto Towers, 525 Collins Street, Melbourne	Rialto South Tower Communications Facility Melbourne, November 2002
The 20.11 hectares of land included in the Games Village project as shown in plan 18698/GV Version A, generally bounded by Oak Street to the east, Park Street to the north, the City Link Freeway to the west and Royal Park to the south-east, excluding the Mental Health Research Institute, in Parkville	The Games Village Project, Parkville, September ~~2006~~ 2015
412-442 Victoria Parade, 167-225 Powlett Street and 148-178 Albert Street, East Melbourne	former Victoria Brewery site, East Melbourne – 'Tribeca' Redevelopment October 2003
Corner Swan Street and Batman Avenue, Melbourne	former Olympic Swimming Stadium, Collingwood Football Club signage, April 2004
Royal Melbourne Showgrounds, Epsom Road, Ascot Vale	Royal Melbourne Showgrounds Redevelopment Project - December 2004
110 Jeffcott Street, West Melbourne (Crown Allotment 19, Section 35, at West Melbourne, Parish of Melbourne North, City of Melbourne)	Judy Lazarus Transition Centre, March 2005
134-144 Southbank Boulevard, 21-43 Sturt Street, and part of Southbank Boulevard, adjacent to the northern boundary of the site, Southbank	Melbourne Recital Hall and MTC Theatre project, August 2005
Land at Princes Park, North Carlton (parkland area generally defined by Royal Parade, Cemetery Road West, Princes Park Drive and the pedestrian path connecting Princes Park Drive and Royal Parade located to the south of Optus Oval)	Big Day Out Music Festival, January 2006
Land comprising public lands and sports and entertainment facilities within the area bounded generally by Swan Street, Punt Road, Batman Avenue and Boulton Parade, Melbourne	Rectangular Pitch Stadium Project: Olympic Park and Gosch's Paddock, Melbourne, August 2007
135-149 Kings Way, Southbank	Advertising Signs - Mercedes-Benz, 135-149 Kings Way, Southbank
Land comprising the Melbourne Convention Centre Development Southbank and associated Northbank redevelopment Docklands, Precinct Plan area, generally bounded by Wurundjeri Way, the north-south alignment of Siddeley Street and its prolongation south to the Yarra River, Charles Grimes Bridge, Montague Street, the southern alignment of the West Gate Freeway, Normanby Road and Clarendon Street, the Yarra River southbank including south wharf and Dukes Dock and Orrs Dock, and parts of the northbank of the Yarra River in the vicinity of the western end of Siddeley Street, and part of the intervening Yarra River	Melbourne Convention Centre Development, Southbank and North Wharf redevelopment, Docklands, April 2006

Address of land	Title of incorporated document
Land comprising the Dynon Port Rail Link Project area generally bounded by Footscray Road (west of the City Link off ramp) and land to the north	Dynon Port Rail Link Project
Land between Yarraville and Doveton adjacent to and encompassing the West Gate Freeway, the City Link Southern Link and Western Link south of the Bolte Bridge, the Monash Freeway and the South Gippsland Freeway, insofar as the land is in the City of Melbourne	M1 Redevelopment Project, October 2006
Land at 167-177 and 181-191 Little Collins Street and 97-101, 103-107 and 109-113 Russell Street. Melbourne	Scots Church Site Redevelopment, Melbourne, May 2013
57-83 Kavanagh Street, Southbank	State Coronial Services Centre Redevelopment Project, August 2007
Land at 47 Whiteman Street, 25-31 Haig Street, 28 Haig Street, 35-39 Haig Street, 57-69 Clarendon Street, 71-77 Clarendon Street and 79-91Clarendon Street, and 93 Clarendon Street, Southbank and roads bounded by the land	Crown Casino Third Hotel, September 2007
Land at 314-336 Bourke Street, 297-309 Little Bourke Street and 315-321 Little Bourke Street, Melbourne	Myer Melbourne Bourke Street store redevelopment, Melbourne, October 2007
The New Royal Children's Hospital Project Area, Flemington Road Parkville, generally bounded by Royal Park, Flemington Road, the tramway and the existing Royal Children's Hospital on the corner of Gatehouse Street and Flemington Road, Parkville	The New Royal Children's Hospital Project, Parkville, October 2007
Land at: 65-71 Haig Street, Southbank 9-15 Moray Street, South Melbourne 1-3 Cobden Street, South Melbourne	Major Promotion Signs, December 2008
Land at: 269, 271-73 and 275-321 Lonsdale Street, 266-78, 280-84, 286-88 and 290-316 Little Bourke Street (including Lynch Place), Melbourne, and Arcade Alley, Melbourne (Corporation Lane 63). The land generally bound by Little Bourke Street to the south, Caledonian Lane to the east, Lonsdale Street to the north and the Strand Central Arcade at 323-345 Lonsdale Street and the Pacific International Apartments building at 318-320 Little Bourke Street to the west	Emporium Melbourne Development, July 2009
Melbourne Park Redevelopment Area - All land bounded generally by Olympic Boulevard between Batman Avenue and Punt Road to the south, Punt Road to the east, the rail corridor and William Barak Bridge to the north and Batman Avenue between William Barak Bridge and Olympic Boulevard to the west. The Area includes the proposed pedestrian bridge alignment and landings over Batman Avenue linking Birrarung Marr and Melbourne Park and the constructed pedestrian bridge over Olympic Boulevard linking Melbourne	Melbourne Park Redevelopment February 2014

Address of land	Title of incorporated document
Park and AAMI Park	
Hamer Hall Redevelopment Area comprising Hamer Hall and land bound by Princes Bridge, St Kilda Road, Yarra River and Southgate Avenue and the Arts Centre lawn	Hamer Hall Redevelopment July 2010
The land required for the Regional Rail Link Project as identified in clause 3 of the incorporated document	Regional Rail Link Project Section 1 Incorporated Document, March 2015
Yarra Park Master Plan Area – The area includes the Yarra Park Reserve, the Melbourne Cricket Ground and the Richmond Cricket Ground/Punt Road Oval in Precinct 1. Precinct 1 is generally bordered by Brunton Avenue to the south, Punt Road to the east, Vale Street South and Vale Street to the north-east, the railway line running parallel to Wellington Parade to the north and Jolimont Street and Jolimont Terrace, East Melbourne, to the west. Precinct 2 on the Area Plan includes part of Wellington Parade South and parts of Wellington Park and Jolimont Reserve that correspond to the alignment of external infrastructure required to connect the source of wastewater to the water recycling facility to be constructed in Yarra Park	Yarra Park Master Plan Implementation September 2010
The '80 Collins Street Development Project' located at the combined property address of 72-74, 76-80, 82 and 84 Collins Street, Melbourne. The land includes Benson Lane, identified as the former Council owned Corporation Lane no.1405, the air space above the former Commercial Bank of Australia at 68-72 Collins Street, Melbourne and the bluestone Lane to the rear of 68-72 Collins Street Melbourne	80 Collins Street Melbourne Development, May 2013
Land on the northern bank of the Yarra River to the east and west of the Charles Grimes Bridge, and over the Yarra River under the Charles Grimes Bridge, as detailed in the "Charles Grimes Bridge Underpass Concept Plan"	Charles Grimes Bridge Underpass, December 2011
Visy Park, Royal Parade, Carlton North	"Visy Park Signage, 2012"
North Melbourne Recreation Reserve, Arden Street, North Melbourne	"North Melbourne Recreation Reserve Signage 2012"
Land north of the Howe Parade reservation corresponding to the indicative alignment of new roads and intersections as detailed in the "Port Capacity Project, Webb Dock Precinct Concept Plan, October 2012"	Port Capacity Project, Webb Dock Precinct, Incorporated Document, October 2012 (Amended August 2016)
Land known as 70 Southbank Boulevard, Southbank, with a site address of 115-221 City Road, across three titles, being Lot 1 and Lot 2 on Plan of Subdivision 334458M and Crown Allotment 45 to 50A, Section 81 of PC354099Q	70 Southbank Blvd, Southbank, June 2014
555 Collins Street, Melbourne	Shadow Controls, 555 Collins Street, Melbourne, February 2013

Address of land	Title of incorporated document
120-130 Southbank Boulevard and 102-118 Sturt Street, Southbank	ABC Melbourne New Office and Studio Accommodation Project (Southbank), December 2013
The Cranbourne Pakenham Rail Corridor Project land as shown on the project area maps and identified in the incorporated document	Cranbourne Pakenham Rail Corridor Project Incorporated Document, September 2014
13-21 Little Lonsdale Street, 261-265 Spring Street and 267-271 Spring Street, Melbourne (part of the land contained in Certificate of Title Volume 10720 Folio 134 and described as Lot 1 on Plan of Subdivision 446765C)	271 Spring Street, Melbourne, Transitional Arrangements, May 2016
433-455 Collins Street Melbourne, (also known as 447 Collins Street Melbourne) contained in Certificate of Title Volume 100043 Folio 738 and being Crown Allotment 15 Section 3 City of Melbourne Parish of Melbourne North	271 Spring Street, Melbourne, Transitional Arrangements, May 2016
The land identified in clause 3 of the Melbourne Metro Rail Project Incorporated Document, December 2016	Melbourne Metro Rail Project Incorporated Document, December 2016
Land affected by Schedule 70 to the Design and Development Overlay	Melbourne Metro Rail Project – Infrastructure Protection Areas Incorporated Document, December 2016
55 Southbank Boulevard, Southbank	55 Southbank Boulevard, Southbank, February 2017
Land at 1-29 Queens Bridge Street, Southbank and land adjacent to these sites described as follows: ▪ 1-7 Queens Bridge Street, Southbank (Lot 1 on Title Plan 369606P and Lot 2 PS332539X) ▪ 9-15 Queens Bridge Street, Southbank (Lot 1 on Title Plan 873768Y) ▪ 17-23 Queens Bridge Street, Southbank (Lot B on Plan of Subdivision 504017Y) ▪ 25-29 Queens Bridge Street, Southbank (Lot 1 of Plan of Subdivision 505293S) ▪ 1-8 Whiteman Street (Allot. 58e City of South Melbourne, Parish of Melbourne South) OP112471 ▪ Queensbridge Square (Allot. 2170 City of South Melbourne, Parish of Melbourne South, Allot. (State) 2168 City of South Melbourne, Parish of Melbourne South (Council)) OP121921A ▪ Queens Bridge Street – Government Road ▪ Sandridge Rail Bridge Allot 2011 Parish of Melbourne North – Council and Allot 2007 Parish Melbourne North – Council ▪ Southbank Boulevard – Government Road ▪ Southbank Promenade Allot 15B City of South Melbourne Parish of Melbourne South - Council	One Queensbridge, 1-29 Queens Bridge Street, Southbank (Crown's Queensbridge Hotel Tower), February 2017

02/03/2017
~~C310~~
Proposed C281

SCHEDULE TO CLAUSE 81.01

Name of document	Introduced by:
271 Spring Street, Melbourne, Transitional Arrangements, May 2016	**C287**
55 Southbank Boulevard, Southbank, February 2017	**C288**
346-376 Queen Street, 334-346 La Trobe Street and 142-171 A'Beckett Street Open Lot Car Park, Melbourne	**NPS1**
447 Collins Street, Melbourne, Transitional Arrangements, May 2016	**C289**
70 Southbank Blvd, June 2014	**C239**
80 Collins Street Melbourne Development, May 2013	**C219**
ABC Melbourne New Office and Studio Accommodation Project (Southbank), December 2013	**C226**
Advertising Signs - Mercedes-Benz, 135-149 Kings Way, Southbank	**C103**
Arden Macaulay Heritage Review 2012: Statements of Significance June 2016	**C207**
Big Day Out Music Festival, January 2006	**C112**
Building Envelope Plan – Replacement Plan No.1, DDO 20 Area 45	**NPS1**
Carlton Brewery Comprehensive Development Plan October 2007	**C126**
Central City (Hoddle Grid) Heritage Review: Statements of Significance June 2013	**C186(Part 1)**
Charles Grimes Bridge Underpass, December 2011	**C191**
City North Heritage Review 2013: Statements of Significance (Revised June 2015)	**C198**
Cliveden Hill Private Hospital, 29 Simpson Street, East Melbourne, July 1999	**C6**
Cranbourne Pakenham Rail Corridor Project Incorporated Document, September 2014	**GC15**
Crown Casino Third Hotel, September 2007	**C136**
David Jones Melbourne City Store Redevelopment, May 2008	**C139**
Dynon Port Rail Link Project	**C113**
Emporium Melbourne Development, July 2009	**C148**
Federation Arch and Sports and Entertainment Precinct Signs, April 2002	**C66**
Fishermans Bend Strategic Framework Plan, July 2014 (amended September 2016)	**GC50**
Flinders Gate car park, Melbourne, July 1999	**C6**
Former Fishmarket Site, Flinders Street Melbourne, September 2002	**C68**
Former Herald and Weekly Times building, 46-74 Flinders Street, Melbourne, August 2002	**C69**
Former Olympic Swimming Stadium, Collingwood Football Club signage, April 2004	**C91**
Former Queen Victoria Hospital Site - Open Lot Car Park, Melbourne	**NPS1**
Former Southern Cross Hotel site, Melbourne, March 2002	**C64**
Former Victoria Brewery site, East Melbourne – 'Tribeca' Redevelopment October 2003	**C86**
Freshwater Place, Southbank, August 2001 (Amended 2012)	**C193**

Name of document	Introduced by:
Hamer Hall Redevelopment July 2010	**C166**
Heritage Places Inventory June2016	**C207**
High wall signs - 766 Elizabeth Street, Carlton	**NPS1**
Hilton on the Park Complex Redevelopment, December 2004	**C101**
Hobsons Road Precinct Incorporated Plan, March 2008	**C124**
Hotham Estate	**C134**
Incorporated Plan Overlay No. 1 – 236-254 St Kilda Road	**NPS1**
Judy Lazarus Transition Centre, March 2005	**C102**
Kensington Heritage Review Statements of Significance October 2014	**C215**
M1 Redevelopment Project, October 2006	**C120**
Major Promotion Signs, December 2008	**C147**
Melbourne Aquarium Signs, July 2001	**C11**
Melbourne Central redevelopment, March 2002	**C62**
Melbourne City Link Project – Advertising Sign Locations, November 2003	**VC20**
Melbourne Convention Centre Development, Southbank and North Wharf redevelopment, Docklands, April 2006	**C116**
Melbourne Girls Grammar – Merton Hall Campus Master Plan, June 2002	**C22**
Melbourne Grammar School Master Plan - Volume One, Senior School South Yarra Campus, Issue Date 14 October 2003.	**C90**
Melbourne Metro Rail Project Incorporated Document, December 2016	**GC45**
Melbourne Metro Rail Project – Infrastructure Protection Areas Incorporated Document, December 2016	**GC45**
Melbourne Park Redevelopment February 2014	**C229**
Melbourne Planning Scheme Incorporated Plan, June 2016, Melbourne Water Permit Exemptions to the Schedule to Clause 43.01 for the Moonee Ponds Creek (HO1092)	**C207**
Melbourne Recital Hall and MTC Theatre project , August 2005	**C111**
Mirvac, Residential Towers, 236-254 St. Kilda Road, Southbank	**NPS1**
Moonee Ponds Creek Concept Plan	**C134**
Myer Melbourne Bourke Street store redevelopment, Melbourne, October 2007	**C137**
North Melbourne Recreation Reserve Signage, 2012	**C172**
North West Corner of Mark and Melrose Street, North Melbourne	**C134**
One Queensbridge, 1-29 Queens Bridge Street, Southbank (Crown's Queensbridge Hotel Tower), February 2017	**C310**
Port Capacity Project, Webb Dock Precinct, Incorporated Document, October 2012 (Amended August 2016)	**GC54**
Promotional Panel sign, Crown Allotment 21D, Power Street, Southbank, July 1999	**C6**
Rectangular Pitch Stadium Project: Olympic Park and Gosch's Paddock, Melbourne, August 2007	**C130**
Regional Rail Link Project Section 1 Incorporated Document, March 2015	**GC26**

Name of document	Introduced by:
Rialto South Tower Communications Facility Melbourne, November 2002	**C57**
Royal Melbourne Showgrounds Redevelopment Master Plan – December 2004	**C100**
Royal Melbourne Showgrounds Redevelopment Project – December 2004	**C100**
Scots Church Site Redevelopment, Melbourne, May 2013	**C202**
Shadow Controls, 555 Collins Street, Melbourne, February 2013	**C216**
Shrine of Remembrance Vista Control April 2014	**C220**
Simplot Australia head office, Kensington, October 2001	**C52**
Sky sign - 42 Clarendon Street, South Melbourne	**NPS1**
Spencer Street Station redevelopment, June 2013	**C218**
Sports and Entertainment Precinct, Melbourne, August 2007	**C130**
State Coronial Services Centre Redevelopment Project, August 2007	**C130**
State Netball and Hockey Centre, Brens Drive Royal Park, Parkville, May 2000	**C26**
The Games Village Project, Parkville, September ~~2006~~ 2015	**C~~115~~281**
The New Royal Children's Hospital Project, Parkville, October 2007	**C128**
Tram Route 109 Disability Discrimination Act compliant Platform Tram Stops, August 2007	**C130**
University of Melbourne Bio 21 Project Parkville, July 2015	**C261**
University of Melbourne, University Square Campus, Carlton, November 1999	**C17**
Visy Park Signage, 2012	**C172**
Yarra Park Master Plan Implementation September 2010	**C158**
Young and Jackson's Hotel, Promotional Panel Sky sign, Melbourne, July 1999	**C6**

MELBOURNE PLANNING SCHEME

Incorporated Document

THE GAMES VILLAGE PROJECT, PARKVILLE, SEPTEMBER 2015~~06~~

Site Description

The 20.11 hectares of land included in the Games Village project as shown in plan 18698/GV version A, generally bounded by Oak Street to the east, Park Street to the north, the City Link Freeway to the west and Royal Park to the south-east, excluding the Mental Health Research Institute, in Parkville.

This document is an incorporated document in the Melbourne Planning Scheme, pursuant to Section 6(2)(j) of the *Planning and Environment Act 1987* (Vic).

1.0 INTRODUCTION

This document is an incorporated document in the schedule to clauses 52.03 and 81 of the Melbourne Planning Scheme.

This incorporated document applies to all of the Land shaded on the plan in clause 16 of this incorporated document.

The controls and provisions in the Melbourne Planning Scheme, other than this incorporated document, do not apply to the subdivision, use or development of the Land for any purpose associated with the Games Village project, except for clauses 54, 55 and 56 of the Melbourne Planning Scheme, which apply only to the extent provided for in this incorporated document.

2.0 PURPOSE

The purpose of this incorporated document is:

- to facilitate the use and development of the Land as the Commonwealth Games Athletes Village for the Commonwealth Games in March 2006
- to ensure the orderly and timely provision of facilities required for, convenient for, or ancillary to, the hosting of the Commonwealth Games
- to allow for the development of the Games Village project in the period before, during and after the Commonwealth Games.

3.0 DEFINITIONS

In this incorporated document:

- "Commonwealth Games", "facilities", and "Games Village project" have the same meaning as in the *Commonwealth Games Arrangements Act 2001* (Vic). For the purposes of determining whether a use, development or subdivision is part of the "Games Village project", the responsible authority may refer to the Project Delivery Agreement between the State and the 'developer'.
- "Land" means the 20.11 hectares of land included in the Games Village project shown in Plan 18698/GV Version A in clause 16 of this incorporated document; and
- any reference to "Minister" means:
  - the Minister for Planning or such other Minister as may from time to time administer section 48B of the *Commonwealth Games Arrangements Act 2001* (Vic) in the period until the expiry of this incorporated document.

4.0 ADDRESS OF THE LAND

This incorporated document applies to the Land. The Land is generally bounded by Oak Street to the east, Park Street to the north, the City Link Freeway to the west and Royal Park to the south-east, excluding the Mental Health Research Institute, in Parkville.

5.0 SUBDIVISION, USE AND DEVELOPMENT ALLOWED UNDER THIS DOCUMENT

This incorporated document allows the subdivision, use or development of the Land without a permit for any purpose associated with the construction and operation of the Games Village project, subject to the subdivision, use or development being in accordance with this incorporated document.

Any use or development of the Land for any purpose not associated with the Games Village project may be allowed by a permit granted under clause 13 of this incorporated document.

No subdivision, use or development of the Land may take place other than in accordance with this incorporated document.

Any consent, approval or permit given or granted under this incorporated document may be given subject to any conditions the decision maker deems fit.

6.0 STAGED DEVELOPMENT AND WAIVER OF REQUIREMENTS

Despite any provision of this incorporated document, the Minister may:

- waive, vary or defer the requirement to prepare an integrated plan or siting and design guidelines under clauses 8 or 9 of this incorporated document, or any requirement as to the content of such plan on such terms as the Minister deems fit, and to grant any consent or give any approval under this incorporated document notwithstanding such waiver, variation or deferment.
- approve any integrated plan under clause 8 of this incorporated document in stages, provided the Minister is satisfied that the plan for the individual stage integrates, or is capable of integrating, with the whole of the Land.
- grant any consent or give any approval under clauses 11, 12 or 13 of this incorporated document for the demolition, subdivision, use or development of the Land in stages.
- grant any consent or give any approval under this incorporated document that extends beyond the expiry of this incorporated document for a specified period not exceeding a period which would provide for the substantial commencement of the use or development by the expiry date or completion by 31 December 2021.

7.0 MASTER PLAN

Before the Minister may approve any plan required under clause 8 of this incorporated document, siting and design guidelines under clause 9 of this incorporated document or provide any approval, consent or permit under this incorporated document, other than a consent under clause 11, a master plan must be prepared that is generally in accordance with the indicative site layout plan in clause 15 of this incorporated document to the satisfaction of the Minister. The master plan must:

- identify all buildings that may be demolished
- provide details of the indicative staging and timing of development
- describe the possible indicative use and development of each part of the Land, and identify the general location of commercial, retail and community facilities and the location of public open space

- include a site layout plan, generally in accordance with the indicative site layout plan in clause 15 of this incorporated document, that adopts a primary north-south road network layout that promotes the integration of the site with surrounding communities and parkland and identifies all footpaths and cycling paths and lanes and public transport routes and facilities, all of which must be located to encourage non-car transport. The path and road widths and features must accord with the design objectives in clause 10 of this incorporated document
- provide for the use of the part of the Land immediately south of the Mental Health Research Institute and the part of the Land fronting Oak Street for a mix of uses including residential, commercial, community, retail and café uses
- provide for the use of the part of the Land in the heritage precinct for a mix of uses including residential, commercial, community (eg aged care and child care facilities), recreation (eg gymnasium), retail and café
- provide an open space corridor which generally links the north and south of the site and links the site with Royal Park to the south and provides a wedge between the apartments on the western boundary of the site and the houses and townhouses to the east of the open space corridor
- provide other areas of open space, including the part of the Land immediately to the south of the Mental Health Research Institute
- provide a tree retention plan
- provide for an aged care facility in the aged care precinct in the north west corner of the site not exceeding four habitable storeys in height
- provide for at least 900 dwellings comprising a modulated line of freeway apartments on the western boundary (not exceeding nineteen~~eleven~~ habitable storeys in height) and a mix of houses, townhouses and other apartments elsewhere on the site
- describe indicative building envelopes and the proposed number of habitable storeys for apartment buildings and the aged care facility. The habitable storeys do not include car parking levels, roof mounted building services, architectural features and similar structures.

The master plan may be amended to the satisfaction of the Minister at any time.

The Minister may only approve a plan under the conditions of this incorporated document that is generally in accordance with the approved master plan, except for a plan prepared for the purposes of demolition or site preparation works under clause 11 of the incorporated document.

7.1 Decision guidelines

The Minister must, in considering whether to approve the master plan to the Minister's satisfaction under this incorporated document, have regard to the following matters:

- the indicative site layout plan in clause 15 of this incorporated document;
- the need to ensure the orderly and timely provision of facilities required for, convenient for, or ancillary to, the hosting of the Commonwealth Games;
- the extent to which any demolition, subdivision, use or development proposed in the master plan will impact upon any land or buildings in the heritage precinct or trees to be retained;
- the design objectives in clause 10 of this incorporated document, as appropriate; and

- the extent to which the use or development proposed in the master plan will facilitate, or impact upon, the development or re-development and use of the Land as part of the Games Village project after the Commonwealth Games.

8.0 INTEGRATED PLANS

Before the Minister may give any approval or consent under this incorporated document, the applicant for approval or consent must prepare the integrated plans in this clause 8 to the satisfaction of the Minister.

For the purposes of clause 12.1, the Minister must not approve a plan of subdivision until after the Minister has approved the integrated plans in clauses 8.1 to 8.4.

For the purposes of clause 13.1, the Minister must not consent to a use or development, except for the use or development of the Land for a display home or display village and sales information centre, until after the Minister has approved all the relevant integrated plans in clause 8.

The plans prepared under this clause 8 must address the whole of the Land in an integrated manner to the satisfaction of the Minister.

Any of the integrated plans prepared under this clause 8 of this incorporated document may be amended to the satisfaction of the Minister at any time.

8.1 Traffic management plan

The traffic management plan must:

- show the internal road layout and circulation including an identified road hierarchy, proposed cross sections for road reserves, road designs, physical traffic management devices (if required), the location and treatment of connection points to the external road network and anticipated flows and volumes of traffic
- show the proposed public transport routes
- show the indicative location of pedestrian and cycling paths or lanes and links to adjoining land and networks
- promote Oak Street/Manningham Street as the priority route for cyclists and pedestrians to access public transport
- adopt the relevant design objectives in clause 10 of this incorporated document.

8.2 Landscape plan

The landscape plan must indicate areas of public open space, retained trees, proposed facilities (including barbeques, tables and benches, children's playground equipment etc), planting themes, fence details and a management and maintenance regime and must:

- identify proposed linkages between the open space network and the pedestrian paths within the Land to adjoining parklands
- ensure no net loss of trees on the site, and identify those trees to be retained
- include tree protection guidelines to protect the trees to be retained
- provide for the selection of vegetation which will integrate with vegetation in surrounding areas, enhance flora and fauna diversity and will include a majority of

species native to Australia with low water use, including species indigenous to northern Melbourne

- minimise irrigation and chemical control, adopt water sensitive design techniques and complement the integrated water management plan
- address the long term maintenance of the landscape design and components
- encourage the installation of energy efficient outdoor lighting in public areas with minimal light spill
- retain the group of River Red Gums on the corner of Oak and Park Streets with no pedestrian paths amongst them
- recreate a small River Red Gum community with a wide range of native and indigenous species that reflects the original ecosystem.

8.3 Integrated water management plan

The integrated water management plan, prepared in conjunction with Melbourne Water, must:

- incorporate reduced water consumption, stormwater management and greywater recycling
- aim to achieve greater reductions in phosphorus, suspended solids, nitrogen, hydrocarbons and litter relative to stormwater quality targets specified in the Urban Stormwater Best Practice Environmental Management Guidelines (1999)
- aim to significantly reduce potable water use below 2000 metropolitan Melbourne per capita water levels
- use MUSIC modelling to calculate stormwater quantity and quality options
- provide for irrigation of public open space from recycled water
- provide for the treatment of stormwater and greywater on site before reuse or discharge
- adopt water sensitive urban design techniques.

8.4 Construction management plan

The construction management plan must:

- provide details of access arrangements to the site
- include hours of operation
- provide a strategy to manage vehicle parking for building contractors and employees to minimise the impact on surrounding uses
- provide details of the stormwater management system to be installed during construction
- implement the practices described in the EPA publication "*Protecting Stormwater Quality for Building Construction Sites*"
- include appropriate techniques for dust control
- address building waste management

- implement the relevant actions of the environmental management plan approved under clause 8.6 of this incorporated document
- provide a communication strategy directed at surrounding residents and uses.

8.5 Community plan

The community plan, must:

- identify proposed community facilities to be provided based on an audit of the capacity of existing facilities in the locality and a needs assessment for the Games Village project and surrounding communities
- propose funding, management and maintenance arrangements for any recommended community and recreation facilities
- recommend community and recreation facilities on the Land that can be accessed by the broader community as well as Games village residents. Recreation facilities provided in individual apartment complexes may be restricted to use by occupants of those apartments
- demonstrate how public transport access will be provided for the community living in the Games Village project
- investigate the modification of retained buildings and construction of new buildings or facilities for community and recreation use on the Land, particularly in the mixed use precincts.

8.6 Environmental management plan

The environmental management plan must address each stage of demolition, design, construction and operation of the Games Village project, and must:

- support and implement the objectives of the integrated water management plan
- support and implement the landscape plan
- incorporate energy efficiency objectives in the design of the Games Village project, including the energy efficiency ratings for dwellings referred to in the design objectives in clause 10 of this incorporated document
- investigate the possibility of providing the Games Village project with renewable energy
- aim to achieve effective waste management, resource recovery and reuse of materials during the demolition and construction phases of the Games Village project in consultation with EcoRecycle Victoria
- provide an appropriate waste management, tracking, monitoring and reporting system
- incorporate a waste management and litter reduction plan, that encourages waste separation and recycling by residents and visitors
- encourage the use of sustainable materials in the construction of the Games Village project
- set a target of recycling 95% of all demolition and construction waste
- provide for information to be given to prospective purchasers about the environmental features of the Games Village project.

8.7 Heritage precinct plan

The heritage precinct plan must be prepared in consultation with a qualified heritage architect for the part of the Land in the heritage precinct and must:

- include an urban design framework to provide detailed guidance for the development of the heritage precinct. The urban design framework must address sightlines and appropriate building heights and setbacks
- identify heritage buildings to be retained and reused
- incorporate heritage conservation guidelines for the adaptive reuse of the buildings and infill development, which allows for new development within the context of the heritage values of the precinct
- delineate public and private open space in front of the heritage buildings and include measures to make this delineation clear to the public
- show any proposed external modification of buildings in the heritage precinct for a mix of uses, including residential, commercial, institutional and recreational uses
- consider the requirements for car parking and the impact of car parking facilities on the heritage values
- include a landscape theme for the heritage precinct
- provide for the creation of an appropriate contemporary "marker" that visually ties the flanking ward buildings together
- adopt the relevant design objectives in clause 10 of this incorporated document.

8.8 Parking precinct plan

The parking precinct plan must:

- implement the recommendations of a car parking study to determine the appropriate car parking provision rates for residents and visitors for a range of uses in the context of public transport and on street car parking availability
- provide for the installation of secure or lockable bicycle storage generally in accordance with the following levels:
  - 1 space per dwelling; plus
  - 0.25 spaces per 100 square metres for apartment visitor use; plus
  - 5 spaces per 1000 square metres for non-residential uses.

8.9 Games mode plan

The Games mode plan must describe the proposed use, layout and access arrangements for the Land during the Commonwealth Games in text and in a plan.

8.10 Decision guidelines

The Minister must, in considering whether to approve any integrated plan under clause 8 of this incorporated document, have regard to the following matters:

- the need to ensure the orderly and timely provision of facilities required for, convenient for, or ancillary to, the hosting of the Commonwealth Games
- the approved master plan under clause 7 of this incorporated document

- the extent to which any use or development proposed under the integrated plan will impact upon any land or buildings in the heritage precinct or trees to be retained
- the design objectives in clause 10 of this incorporated document, as appropriate
- the extent to which the use or development proposed under the integrated plans will facilitate, or impact upon, the development or re-development and use of the Land as part of the Games Village project after the Commonwealth Games
- where the development proposed under the integrated plan will form part of, or be later modified or re-developed as part of, the Games Village project after the Commonwealth Games, the extent to which the development complies with, or can be later modified or re-developed to be generally consistent with the objectives and principles in clauses 54, 55 and 56 of the Melbourne Planning Scheme, where relevant.

9.0 SITING AND DESIGN GUIDELINES

Before any development is commenced or any application for the consent of the Minister for any development under clause 13 of this incorporated document is made in the:

- houses and townhouses precinct
- freeway apartment precinct
- other apartment precincts
- aged care precinct
- mixed use precinct,

as shown on the indicative site layout plan in clause 15 of this incorporated document, siting and design guidelines must be prepared and approved to the satisfaction of the Minister for that precinct.

The siting and design guidelines must address, as appropriate for each precinct:

- relevant design objectives for built form in each precinct, including height, setbacks, massing and roof form
- materials and finishes
- design and integration of building services including environmental features
- fencing
- location of garages and carports
- acoustic performance in habitable rooms
- accessibility requirements
- views from key vantage points external to the site including the CityLink Freeway, Travencore, Royal Park, Oak Street and Park Street
- the applicable design objectives in clause 10 of this incorporated document.

10.0 DESIGN OBJECTIVES

10.1 For the overall development

- To encourage the provision of accommodation suitable for older members of the community in addition to the aged care facility and the social housing.
- To provide at least 20% of housing with internal layouts that can be modified or adapted at a later date to enable wheelchair access.
- To ensure that dwellings allocated for social housing are designed and constructed to be visually integrated with the surrounding development.
- To design all dwellings to meet the appropriate noise standards
- To protect the amenity of adjacent residential uses with respect to overlooking, privacy, access to sunlight, access to daylight for habitable room windows, useable private open space, visual bulk and opportunities for solar collection and passive solar access.
- To incorporate measures where necessary to manage the impact of non-residential uses on the amenity of residential uses.
- To ensure that roads to be used by public buses are designed to accommodate low floor buses.
- To provide footpaths that are at least 1.5 metres wide.
- To design roads where required to accommodate bus access, adequate on-street parking, service vehicles and bicycle lanes.
- To provide disabled and safe access to public areas, including the road and path network.
- To provide disabled access to all houses, townhouses and apartments (where lifted).
- To incorporate water sensitive design techniques into the design of roads, landscaping, public open space and other developments having regard to the integrated water management plan and stormwater management systems for the site.
- To protect the structural integrity and access to the Royal Park Main Drain, the Moonee Ponds Sewer Main and the Moonee Ponds Sewer Main Deviation.
- To acknowledge the pre-settlement cultural heritage of the Land in the development of the Village (landscaping, public art or public places).
- To encourage the use of sustainable building materials, including plantation timbers for houses and townhouses.
- To avoid uniform development and encourage diversity of design.
- To provide high quality residential development including integrated social housing, community facilities and public open space.
- To apply the principles of sustainable development.
- To adapt and re-use the retained heritage buildings.
- To protect trees identified for retention.
- To create an attractive, park-like setting.
- To create a safe, pedestrian oriented, primarily residential environment that integrates with the surrounding area, particularly the residential neighbourhood to the north.

- To encourage the use of public transport.
- To develop a community hub with a mix of uses, including residential use, within the heritage precinct and the proposed neighbourhood centre on Oak Street.
- To provide attractive, useable public open space that caters for a range of age groups and provides for a variety of recreational experiences.
- To provide appropriate built form, landscape treatment and interface conditions along Oak Street, Park Street and City Link and with other uses that adjoin the Land.
- To integrate the Games Village project with surrounding communities and parkland.
- To provide appropriate traffic management works and techniques in order to accommodate additional traffic generated by the development.
- To implement the recommendations of an accessibility audit.
- To implement the recommendations of a road safety audit.
- To encourage the use of public transport, walking and cycling, including encouraging the use of the Flemington Bridge public transport node.
- To implement the integrated plans approved under clause 8 of this incorporated document.

10.2 For houses and townhouses

- To achieve a high quality residential development with appropriate siting and design guidelines as approved under clause 9.
- To aim to achieve a six star energy efficiency rating, as rated by First Rate Software.
- To install gas boosted solar hot water systems.
- To aim to achieve a minimum 60% solar contribution in hot water systems.
- To orientate lots and dwellings to take advantage of solar access.

10.3 For apartments generally

- To design apartment buildings that respond to their context taking into account sightlines external to the site.
- To provide appropriate access to the apartments for service, emergency and delivery vehicles and adequate parking for tradespeople and removalists.
- To design all apartments to achieve the minimum energy efficiency rating prescribed at the time of construction, and to aim to achieve an overall energy efficiency rating for the apartments higher than the minimum prescribed.
- To promote high quality apartment developments that make a positive contribution to the built form of the area, provide architecturally interesting facades and provide a high level of internal amenity for residents.
- To conceal external plumbing pipes and fixtures, excluding downpipes.
- To integrate roof-mounted structures into the design of the buildings.
- To provide a diversity of architectural expression in the design of the apartment buildings.
- To provide a clearly identified pedestrian entry at street level.

- To provide adequate, safe and efficient car parking for residents and visitors. Car parking for residents should not dominate the streetscape.
- To provide bicycle storage facilities for residents and visitors in accordance with the rates set out in the parking precinct plan under clause 8.8 of this incorporated document.
- To achieve an average apartment height of approximately six habitable storeys.

10.4 Freeway apartments

In addition to the general apartment design objectives in clause 10.3, above:

- To create an apartment layout along the western boundary of the site that is dynamic, articulated curvilinear and highly modulated and enhances the existing Melbourne Gateway.
- To create a modulated layout from three habitable storeys up to, but not exceeding, ~~eleven~~ nineteen habitable storeys in height with the tallest elements representing urban markers for the Games Village project.
- To restrict the apartment buildings adjacent to the retained heritage buildings within the heritage precinct to a maximum of five habitable storeys in height.
- To develop a high quality apartment at the southern end of the freeway apartment precinct that aims to provide:
  - an acoustic buffer
  - passive surveillance over the wetlands and pedestrian routes
  - an urban marker; and
  - a visual and physical link between the open space network within the Games Village Project to both the wetlands and Royal Park.
- To ensure that the façade of the apartments facing the western boundary of the site is of a high architectural standard and does not include any advertising.
- To minimise any increase in traffic noise in Travencore through the design and use of materials on the western façade.
- To respect the integrity and significance of the retained heritage buildings.
- To reserve three metres of land between the apartment building and the noise wall along City Link to provide access between the wall and the apartment buildings.
- To provide an effective acoustic barrier for the balance of the Land.
- To respect the architectural integrity of the International Gateway Sculpture along the City Link Freeway.

10.5 For the aged care precinct

In addition to the general apartment design objectives in clause 10.3, above:

- To restrict the height of buildings in the aged care precinct to a maximum of four habitable storeys.
- To provide disabled access throughout the aged care precinct.
- To provide convenient access to community and recreation facilities and public transport within the Land.

10.6 For any part of the Land in a mixed use precinct

- To encourage a mix of retail, café, commercial and residential uses.
- To manage the impact of non-residential uses on residential use and to develop appropriate means to mitigate these effects.
- To provide adequate car and bicycle parking for proposed uses within a mixed use precinct.
- To create attractive north-facing public spaces.

10.7 For any part of the Land in the heritage precinct

- To develop the heritage precinct as a fine-grained, closely settled urban precinct generally developed around the existing road alignment to the west of the ward and dining room buildings.
- To encourage more intense infill development around the existing buildings.
- To respect the heritage values of the retained heritage buildings and achieve an appropriate built form outcome.
- To encourage a diverse mix of uses, including residential, community (eg childcare), retail, café, commercial, education and recreation uses.
- To provide an appropriate public open space setting for the retained heritage buildings.

11.0 DEMOLITION, SITE PREPARATION AND MAINTENANCE WORKS

11.1 Works allowed

A permit is not required to undertake:

- routine maintenance or temporary refurbishment works;
- demolition works; or
- site preparation works (including earthworks reasonably required for development on the Land).

The consent of the Minister is required to undertake:

- demolition or site preparation works in the heritage precinct; or
- any site preparation works, if those works are proposed to be undertaken before the Minister has approved the master plan in accordance with clause 7 and the landscape plan, construction management plan and environmental management plan in accordance with clauses 8.2, 8.4 and 8.6.

11.2 Consent application requirements

For demolition or site preparation works in the heritage precinct

Before the Minister consents to any demolition or site preparation works in the heritage precinct, the applicant for consent must prepare, as appropriate, to the satisfaction of the Minister:

- a tree retention plan, including an assessment of the proposed works prepared by a qualified arborist and appropriate tree protection guidelines
- a photographic and historical record to be lodged with the State Library of any building in the heritage precinct to be demolished

- a demolition and site preparation plan, which identifies the buildings to be demolished, the works proposed to be undertaken, details the access arrangements to the site and hours of operations, and includes appropriate techniques for dust control, stormwater management and addresses building waste management.

For site preparation works to be undertaken before the Minister has approved master and integrated plans

Before the Minister consents to any site preparation works on the Land proposed to be undertaken before the Minister has approved the master plan in accordance with clause 7 and the landscape plan, construction management plan and environmental management plan in accordance with clauses 8.2, 8.4 and 8.6, the applicant for consent must prepare the following, as appropriate, to the satisfaction of the Minister:

- a tree retention plan, including an assessment of the proposed works prepared by a qualified arborist and appropriate tree protection guidelines
- an outline construction management plan and outline environment management plan which describe the works proposed to be undertaken, details the access arrangements to the site and hours of operations, and includes appropriate techniques for dust control, stormwater management and addresses building waste management.

11.3 Decision guidelines

The Minister must, in considering whether to give consent or to approve plans to the Minister's satisfaction under this incorporated document, have regard to the following matters:

- the need to ensure the orderly and timely provision of facilities required for, convenient for, or ancillary to, the hosting of the Commonwealth Games
- the extent to which the demolition or site preparation work under this provision will impact upon any or trees identified to be retained
- the extent to which the plans satisfy the requirements of this incorporated document, particularly clauses 8.2, 8.4 and 8.6.

12.0 SUBDIVISION

12.1 Subdivision allowed

A permit is not required to subdivide any part of the Land or building within the Land provided that the plan of subdivision is approved by the Minister.

The Minister must not approve a plan of subdivision under this clause 12, until after the Minister has approved the integrated plans in clauses 8.1 to 8.4, unless the Minister is satisfied that:

- the approval is necessary to facilitate the orderly and timely development of the Commonwealth Games Athletes Village; and
- the subdivision will not prejudice any future subdivision, use or development of the Land from complying with this incorporated document and the objectives of clause 56 of the Melbourne Planning Scheme, where relevant.

12.2 Approval application requirements

Before the Minister approves a plan of subdivision, the applicant for approval must prepare the following information, as appropriate, to the satisfaction of the Minister:

- evidence that the applicant has consulted with the relevant servicing authorities and has made arrangements for the supply of services to all lots created within the Land
- survey plans with dimensions and specifications of subdivision identifying lots, retained trees with tree protection zones, servicing connection points, easements, all roads and reserves
- details of the alignment, design and construction or installation of all roads, footpaths, bicycle paths, drainage and all utility services.

12.3 Decision guidelines

The Minister, in considering whether to approve a plan of a subdivision under this incorporated document, must have regard to the following matters:

- the need to ensure the orderly and timely provision of facilities required for, or ancillary to, the hosting of the Commonwealth Games
- general consistency with the objectives of clause 56 of the Melbourne Planning Scheme
- the extent to which the subdivision accords with the relevant integrated plans, if any, approved by the Minister under clause 8 of this incorporated document
- the extent to which the subdivision will facilitate, or impact on, the development or re-development and use of the Land as part of the Games Village project after the Commonwealth Games
- the design objectives in clause 10 of this incorporated document
- the views of any relevant servicing authority.

12.4 Conditional approval

The Minister may approve the plan of subdivision subject to any terms the Minister deems fit and may impose conditions with respect to, but not limited to, the following:

- works near, and the protection of, retained trees
- details of lighting in public areas and signage
- materials to be used for the construction of roads and paths
- landscaping of road reserves and public open space
- detailed engineering construction plans of public assets
- maintenance
- the location of services to be provided
- conditions required by any servicing authorities
- a requirement for a bond or agreement to secure the provision of services or facilities to be provided after the plan of subdivision is certified or registered.

13.0 USE OR DEVELOPMENT

13.1 Use or development allowed

Permit not required

A permit is not required to use or develop the Land for:

- any purpose associated with the construction and development of the Games Village project;
- the provision of facilities required for, convenient for, or ancillary to, the hosting of the Commonwealth Games;
- the display of a temporary outdoor advertising sign for a specified period expiring on or before 31 December 2006; or
- a display home, display village, or a sales and information centre associated with the Games Village project,

provided the use or development is carried on with the consent of the Minister and in accordance with plans prepared to the satisfaction of the Minister. The use and development must also comply with the conditions of this incorporated document.

Without limiting this provision, a use or development which may be authorised by the Minister without a permit under this provision may include:

- Accommodation, including temporary accommodation for competitors, officials and members of the media which may not otherwise comply with clauses 54 or 55 of the Melbourne Planning Scheme, provided that the temporary accommodation is removed on or before 31 December 2006. All accommodation that may be retained as part of the Games Village project after the Commonwealth Games must comply with the objectives in clauses 54 and 55 of the Melbourne Planning Scheme or be capable of being modified or re-developed to comply with the objectives of those clauses.
- Infrastructure and services, including roads, car parking, transport terminal, warehouse, and utility installation.
- General services for the purposes of the staging of the Commonwealth Games, including temporary catering and entertainment facilities; education centre; place of assembly; office; media and communication facilities; retail premises including food and drink premises; leisure and recreation including training facilities.
- Support services such as a medical centre, child-care centre, mail centre, dry cleaning, laundromat and home occupation.

The Minister may consent to a use or development under this provision being carried on for a specified period or on such terms or conditions as the Minister may deem fit. Any temporary use of the Land for the purpose of staging the Commonwealth Games must cease on or before 31 December 2006.

If the Minister consents to any temporary use of the Land, no existing use right under clause 63 of the Melbourne Planning Scheme is established for that temporary use.

The Minister must not consent to any use or development under this clause 13, until after the Minister has approved the integrated plans in clauses 8.1 to 8.9 and the siting and design guidelines under clause 9 of this incorporated document, unless:

- the use or development is for a display home or display village and sales information centre,

or the Minister is satisfied that:

- the consent is necessary to facilitate the orderly and timely development of the Commonwealth Games Athletes Village; and
- the use or development will not prejudice any future subdivision, use or development of the Land from complying with this incorporated document, and if the development may be retained after 31 December 2006, the objectives of clauses 54, 55 and 56 of the Melbourne Planning Scheme, where relevant.

Permit required

A permit is required for any use or development of the Land not associated with the Games Village project. The consent application requirements in clause 13.2 apply, as appropriate, to an application for a permit for use or development. The Minister, in deciding whether to grant a permit, must consider the decision guidelines in clause 13.3, as appropriate, and must also have regard to:

- whether the use or development would unduly affect the Games Village project; and
- the underlying zoning of the part of the Land the subject of the permit application, and the controls and provisions of the Melbourne Planning Scheme (other than this incorporated document) which would otherwise apply.

If the Minister grants a permit for any temporary use of the Land:

- the temporary use must cease on or before the expiry of this incorporated document; and
- no existing use rights under clause 63 of the Melbourne Planning Scheme are established for that temporary use,

unless the use complies with the controls and provisions of the Melbourne Planning Scheme (other than this incorporated document) after the expiry date.

An application for a permit for a temporary use of the building known as the Clinical Services building is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

13.2 Consent application requirements

Any use or development

Before the Minister consents to or grants a permit for any use or development on the Land, the applicant for consent must prepare the following information, as appropriate, to the satisfaction of the Minister:

- a proposed site layout plan to scale and dimensioned showing the details of the use and development subject to the application
- detailed landscape plans relevant to the specific part of the Land to which the application relates, including details of the area set aside for landscaping, open space, a schedule of proposed vegetation, information on irrigation in accordance with the integrated water management plan, fencing details, lighting, paving, seating and signage

- a statement explaining how the proposal addresses the design objectives and guidelines, if relevant
- specific details of how the particular works comply with any plans approved by the Minister under clause 8 of this incorporated document
- an assessment prepared by a qualified arborist and tree protection guidelines for during and after construction, if buildings and works encroach on a tree protection zone around a retained tree
- existing and proposed levels to Australian Height Datum and the location and extent of cut and fill
- a servicing plan identifying services necessary for the operation of the Land as the Commonwealth Games Athletes Village and those services required for the modification or redevelopment of the Land after the Commonwealth Games as part of the Games Village project.

Particular use or development: display home or display village and sales information centre

An application for the consent of the Minister or for a permit to use or develop the Land as a display home, display village or a sales and information centre must include provision for car parking for staff and visitors and nominate the hours of operation.

Particular use or development: residential development

In addition to information required under any other provision in this incorporated document, any application for residential use or development must include the following information, as appropriate:

- how, if at all, the development will be used for the purposes of the Commonwealth Games
- proposed layout plan to scale and dimensioned including the location of dwellings, access and driveways, the location and area of private open space, garage/car port and boundary set backs
- elevations
- floor plans and architectural plans showing the design and heights of external walls and the overall building measured from natural ground level
- indicative schedule of external materials, finishes and colours
- sill heights of windows measured from the finished floor level
- shadow diagrams for 9 AM, 12 Noon and 3 PM as at 22 September, relative to open space and habitable room windows on adjacent properties
- a statement of the environmental features of the development
- location and provision of bicycle storage at a rate set out in the parking precinct plan approved in accordance with clause 8.8 of this incorporated document
- extent of provision for disabled access
- a development schedule detailing site area, number of dwellings, building site coverage and percentage of impervious surface

- a development schedule detailing for each dwelling the type (1, 2, 3 bedroom), the amount of floor area, number of car spaces, number of bicycle parking spaces, area of private open space.

Particular use or development: apartment dwelling development

In addition to information required under any other provision in this incorporated document, including the requirements for residential development generally, an application for use or development of an apartment building must include the following information, as appropriate:

- a scaled and dimensioned car parking plan showing the number of car parking spaces for residents and visitors, aisle widths, ramps, access points, traffic management devices etc. The information must include a statement justifying the number of car parking spaces provided
- arrangements for the storage and collection of garbage including details of any loading area
- plans showing vehicular and pedestrian ingress and egress points to the building
- details of any external services (eg piping, roof mounted building services), architectural features or similar structures
- an assessment of the impact of the proposal on the amenity of adjacent residential uses with respect to overlooking, access to sunlight, access to daylight for habitable room windows, visual bulk and opportunities for passive solar energy collection.

Particular use or development: freeway apartments

In addition to information required under any other provision in this incorporated document, including the requirements for residential development and apartments generally, an application for use or development of apartments on the western boundary of the site must include the following information, as appropriate:

- an assessment prepared by a qualified architect on the impact of the proposal on the architectural integrity of the International Gateway Sculpture (City Link). The assessment must include a photomontage or other appropriate means to demonstrate the visual impact of the proposal on the Gateway Sculpture and views from Travencore
- a plan showing the proposed treatment of the ground level area between the apartment building and the noise wall along City Link.

Particular use or development: any part of the Land in a mixed use precinct

In addition to information required under any other provision in this incorporated document, an application for use or development of any part of the Land in a mixed use precinct must include the following information, as appropriate:

- a full description of the proposed use
- measures to manage the impact of the proposed non-residential use on the amenity of residential uses
- a full description of the type of use proposed including hours of operation, number of staff and other persons on the premises at any time, signage, external lighting etc relevant to the consideration of the application

- a scaled and dimensioned car and bicycle parking plan in accordance with the parking precinct plan in clause 8.8.

Particular use or development: any part of the Land in the heritage precinct

In addition to information required under any other provision in this incorporated document, an application for use or development of any part of the Land in the heritage precinct must include a statement prepared by a qualified heritage architect explaining how the proposal responds to urban design framework for the heritage precinct and achieves the design objectives in clause 10 of this incorporated document and the guidelines for the heritage precinct identified in the heritage precinct plan approved under clause 8.7.

13.3 Decision guidelines

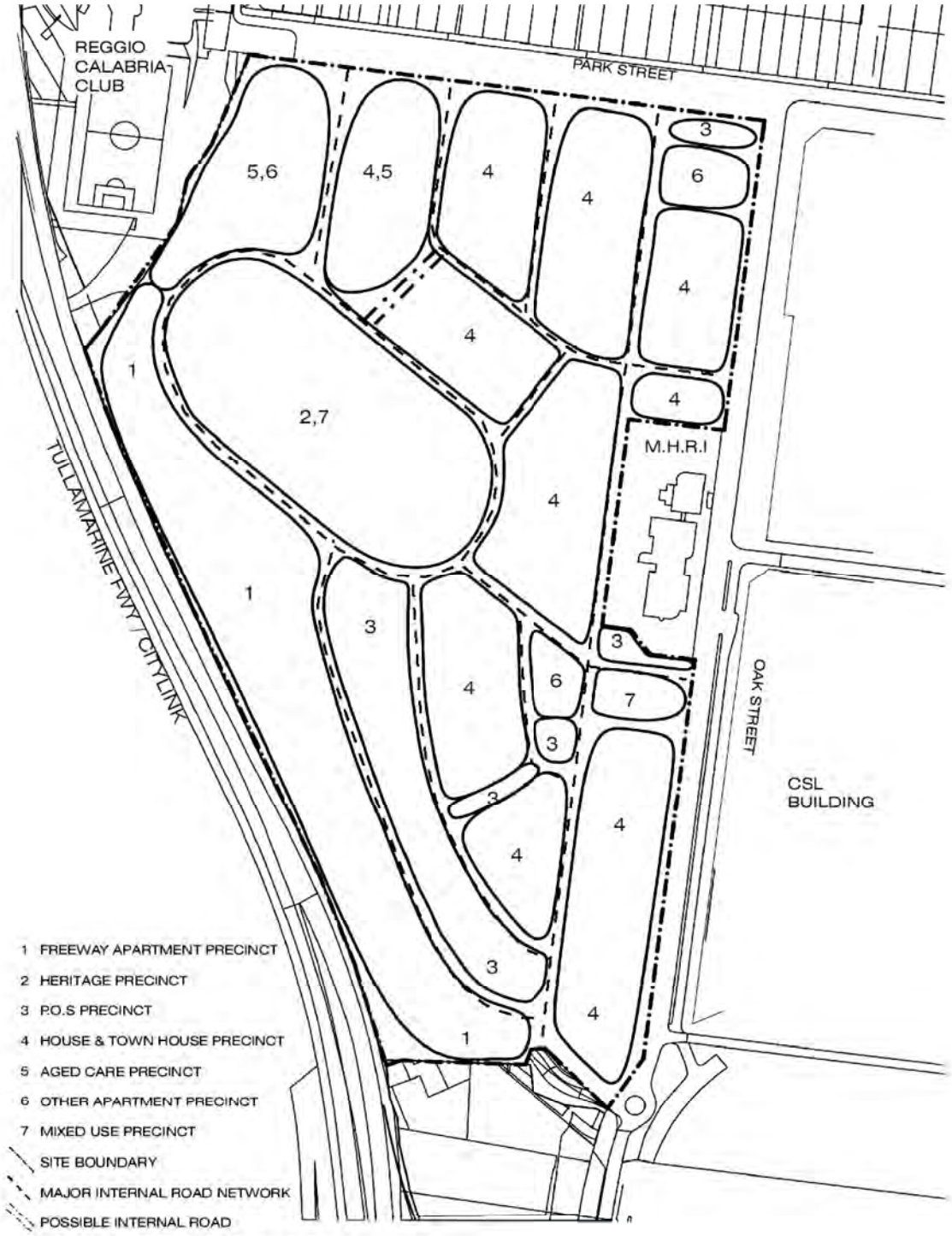
The Minister must, in considering whether to give consent, to grant a permit or to approve plans to the Minister's satisfaction under this clause 13, have regard to the following matters:

- the master plan approved in accordance with clause 7 of this incorporated document
- the integrated plans approved in accordance with clause 8 of this incorporated document
- the siting and design guidelines approved in accordance with clause 9 of this incorporated document
- the design objectives in clause 10 of this incorporated document
- the need to ensure the orderly and timely provision of facilities required for, convenient for, or ancillary to, the hosting of the Commonwealth Games
- the extent to which the use or development under this provision will impact upon any land or buildings in the heritage precinct or trees identified to be retained
- the extent to which the plans satisfy the requirements in the conditions of this incorporated document
- the views of any relevant servicing authority
- the extent to which the use or development authorised under this provision will facilitate, or impact upon, the development or re-development and use of the Land as part of the Games Village project after the Commonwealth Games
- where the development authorised under this provision will form part of, or be later modified or re-developed as part of, the Games Village project after the Commonwealth Games, the extent to which the development complies with, or can be later modified or re-developed to be generally consistent with the objectives and principles in clauses 54, 55 and 56 of the Melbourne Planning Scheme, where relevant.

14.0 EXPIRY

This site specific control under Clause 52.03 expires on 31 December 2018.

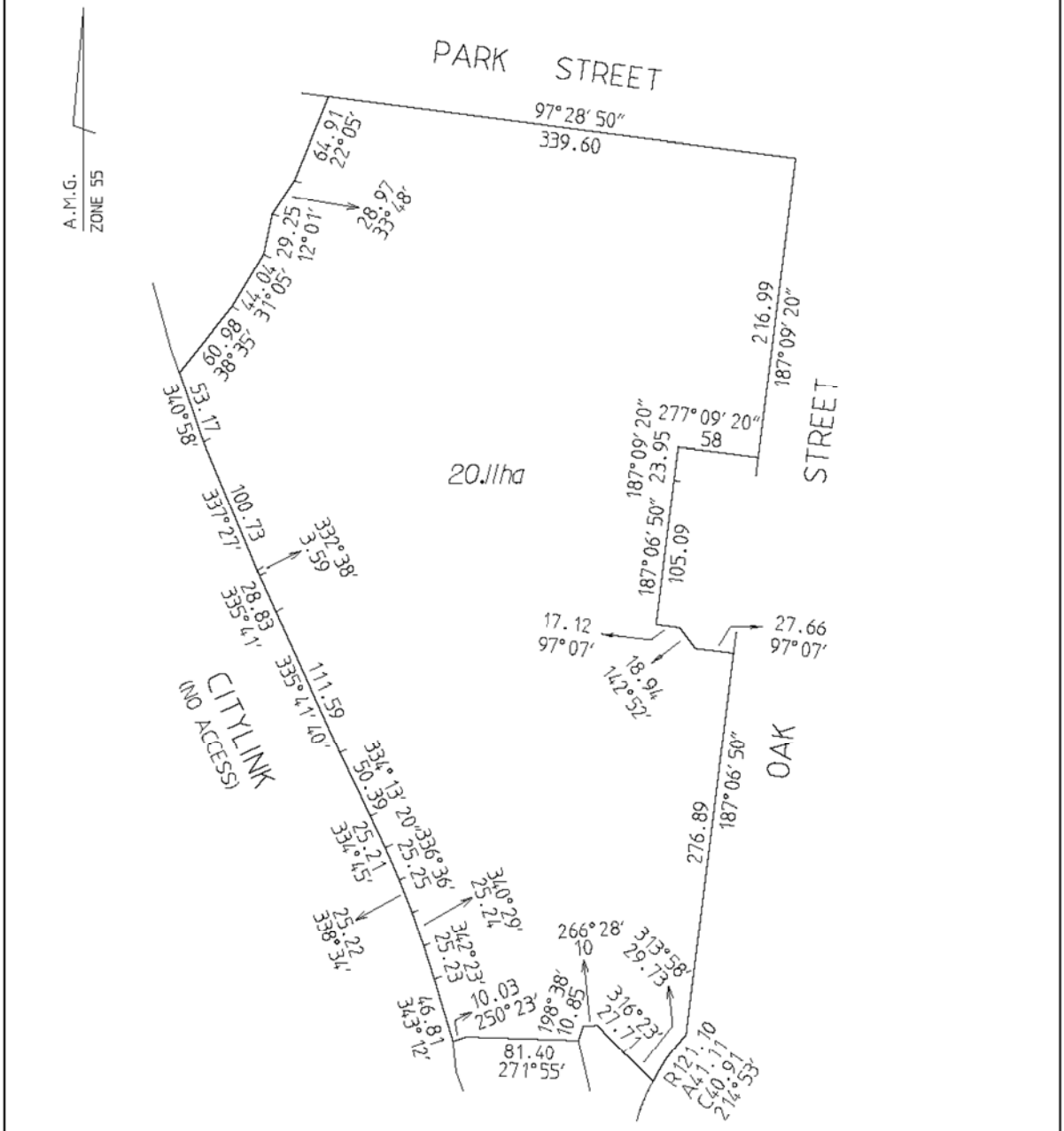
15.0 INDICATIVE SITE LAYOUT PLAN



GAMES VILLAGE PROJECT
INDICATIVE SITE LAYOUT PLAN

16.0 PLAN OF THE GAMES VILLAGE PROJECT LAND

LAND INCLUDED IN GAMES VILLAGE PROJECT



SHEET SIZE	SCALE	REFERENCE: 18698/GV	Reeds Consulting Pty Ltd ACN 679 643 815 ABN 17 251 075 571
A4	1:4000	VERSION: A SHEET 1 of 1	21 Bouverie Street Carlton 3053
LENGTHS ARE IN METRES		DATE: 26/09/03 1SD1A.DGN	phone (03) 9349 4277 fax (03) 9362 2766 e-mail survey@reedscon.com.au

ISO 9001 Lic QEC3254 Standards Australia Quality Endorsed Company

REEDS CONSULTING

Supporting documents

FRASERS PROPERTY

citta property group

VILLAGE PARK

Commonwealth Games Village 2006

PLANNING CONSENT SUBMISSION MASTERPLAN

07th September 2015

TABLE OF CONTENTS

ACKNOWLEDGEMENTS

Village Park Consortium (VPC)
- Australand
- Citta Property Group

BLP+SJB Architects
- Billard Leece Partnership
- SJB Architects Melbourne

Tract Consultants
Tree Logic
Reeds Consulting
Grogan Richards

INTRODUCTION

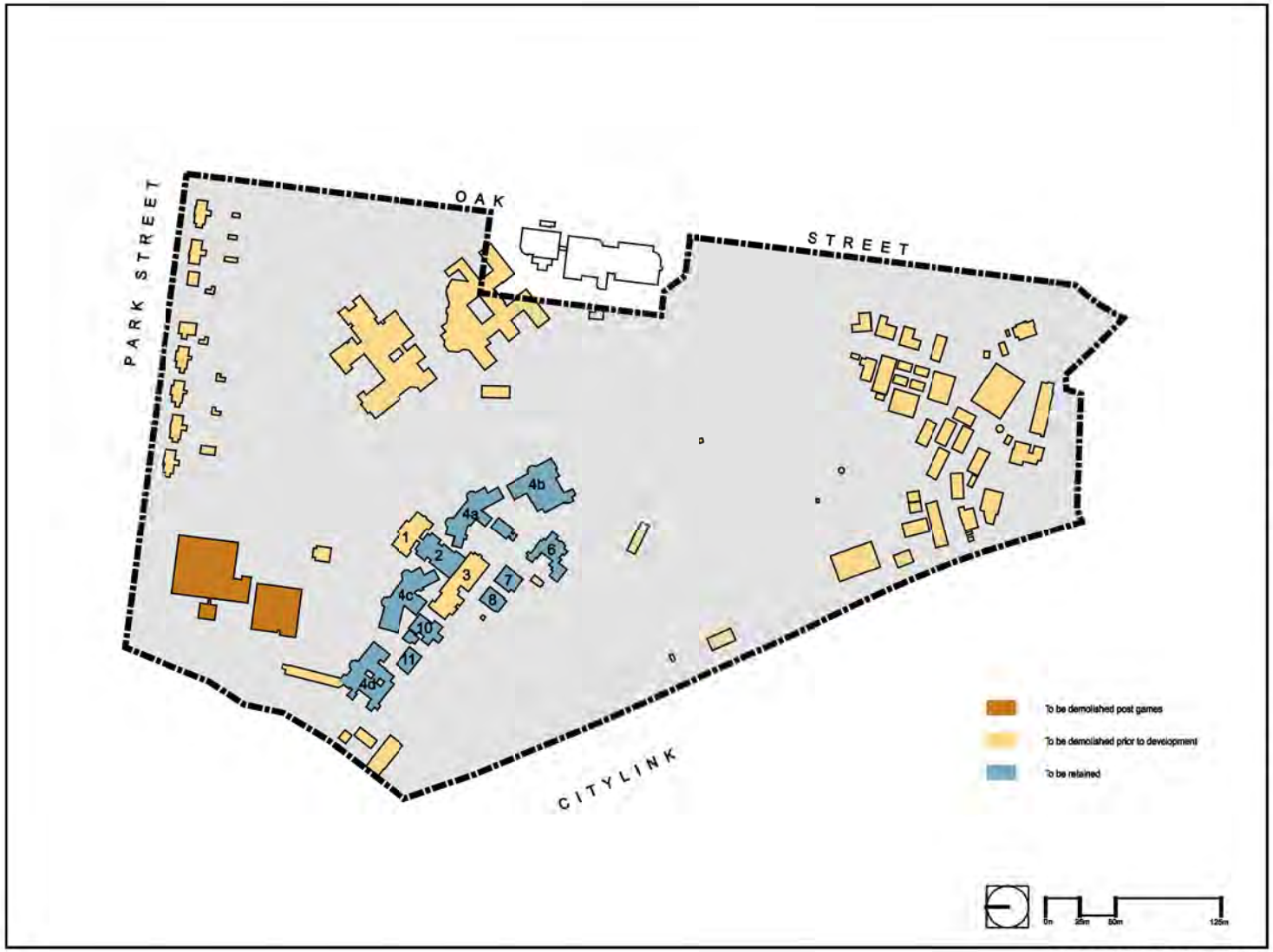
This document has been prepared in accordance with the requirements of the Melbourne Planning Scheme Incorporated Document. (Site specific control - The Games Village Project; Parkville) and in particular to Clause 7.0 "Masterplan".

MASTERPLAN





1.0



DEMOLITION PLAN

Non-heritage Buildings

Non-heritage buildings previously on the site include:

- Disused buildings including the former clinical services building associated with the Royal Park Psychiatric Hospital
- Dwellings along the northern Park Street boundary
- The Northern College of TAFE Horticultural College located in the southern part of the site;
- The Milparinka Day Training Centre

All of these buildings (excluding the Clinical Services Building) have been demolished.

The Clinical services building is to be used as part of the support infrastructure for the athletes village before and during the Commonwealth Games in 2006. It will be demolished post Games to allow for future development.

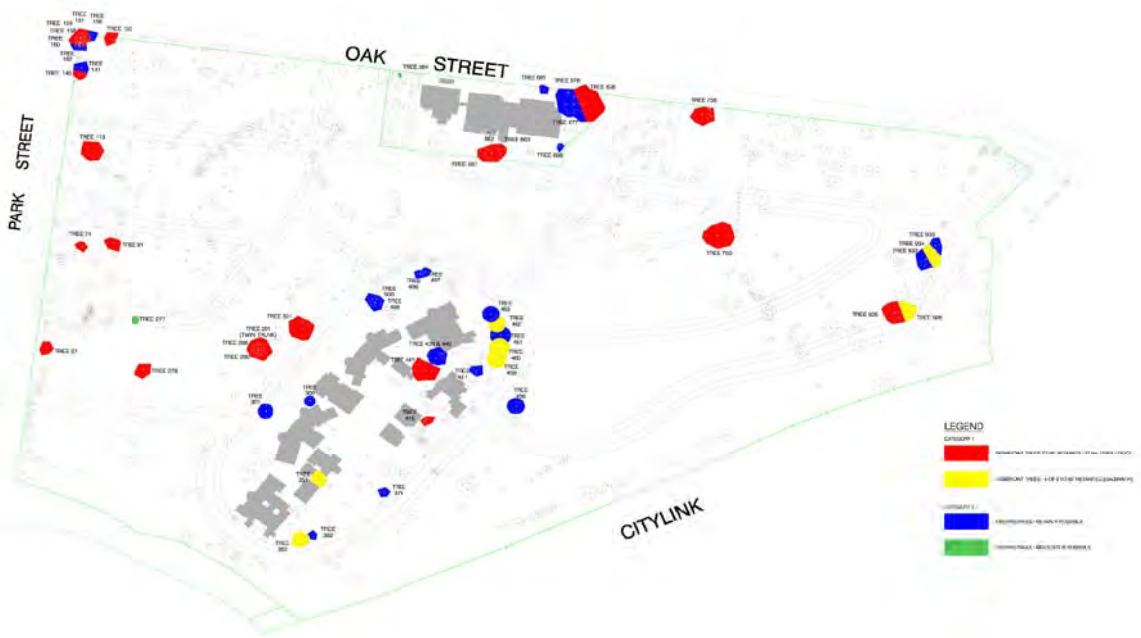
Heritage Buildings

Major Projects Victoria has demolished certain buildings within the Village in order to provide opportunities for infill development and fulfil the State's obligations with respect to the handover of the site to VPC. The buildings that have been demolished include:

- Administration Block (Building 1)
- General Store and Wash House (Building 3)
- Female Convalescent Ward, 1923 Wing (Building 5)
- Boiler House (Building 12)
- Engineering Workshop (Building 13)

The remaining buildings, comprising Buildings 2, 4a, 4b, 4c, 4d, 4e, 6, 7, 8, 10 and 11, are to be retained for residential and community uses with exception of Building 9, the former 'Paint Store' which will be demolished prior to the Games in accordance with the Heritage Precinct Plan.

1.02 TREE RETENTION PLAN



TREE RETENTION PLAN

Category 1

Category 1 includes 31 trees that will be retained by VPC. Category 1 comprises of two sub-categories:

a) Trees identified by the arbicultural consultants, Treelogic as having the highest retention value (significant trees) will be retained in their current location. These trees are shown in red on the attached plan.

b) Trees identified by the arbicultural consultant, Galbraith & Associates as worthy of retaining. These trees are shown in yellow on the attached plan.

Category 2

VPC has identified a number of additional trees which would be preferable to retain or relocate if possible. Category 2 trees also broadly comprise of two sub-categories:

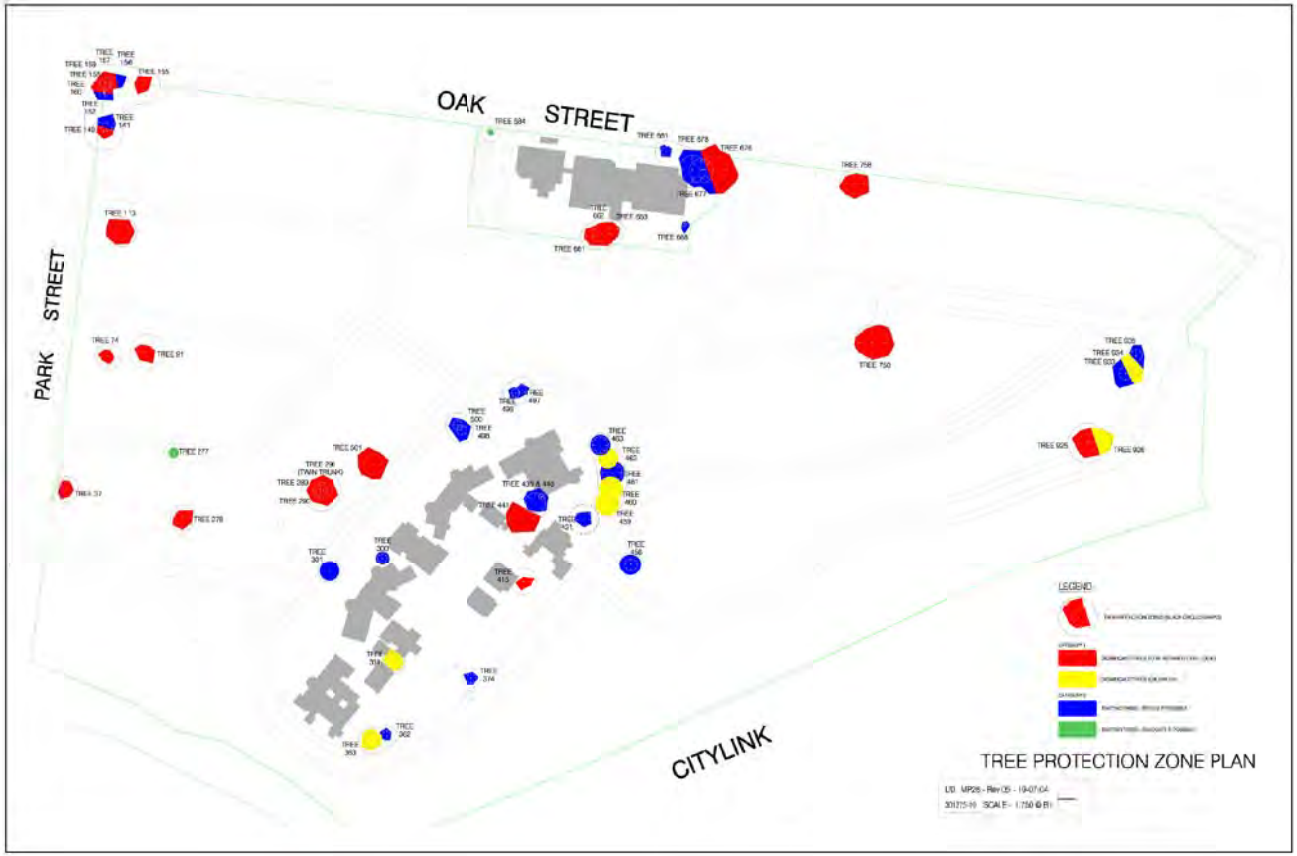
a) Trees that may be possible to retain in place – Trees that could be retained in their current location if they remain in good health and are suitably located (ie consistent with the Games Mode and long-term development of the Village). These trees are generally not suitable for transplanting and are shown in blue on the attached plan.

b) Trees that may be possible to relocate - Trees that could be retained in their current location or transplanted if they remain in good health, are suitably located, or can be transplanted to a suitable location within the Village. Root preparation may be carried out to facilitate the relocation of trees in this category and loss of tree(s) may result if this cannot be carried out in time to achieve the program of works. These trees are shown in green on the attached plan.

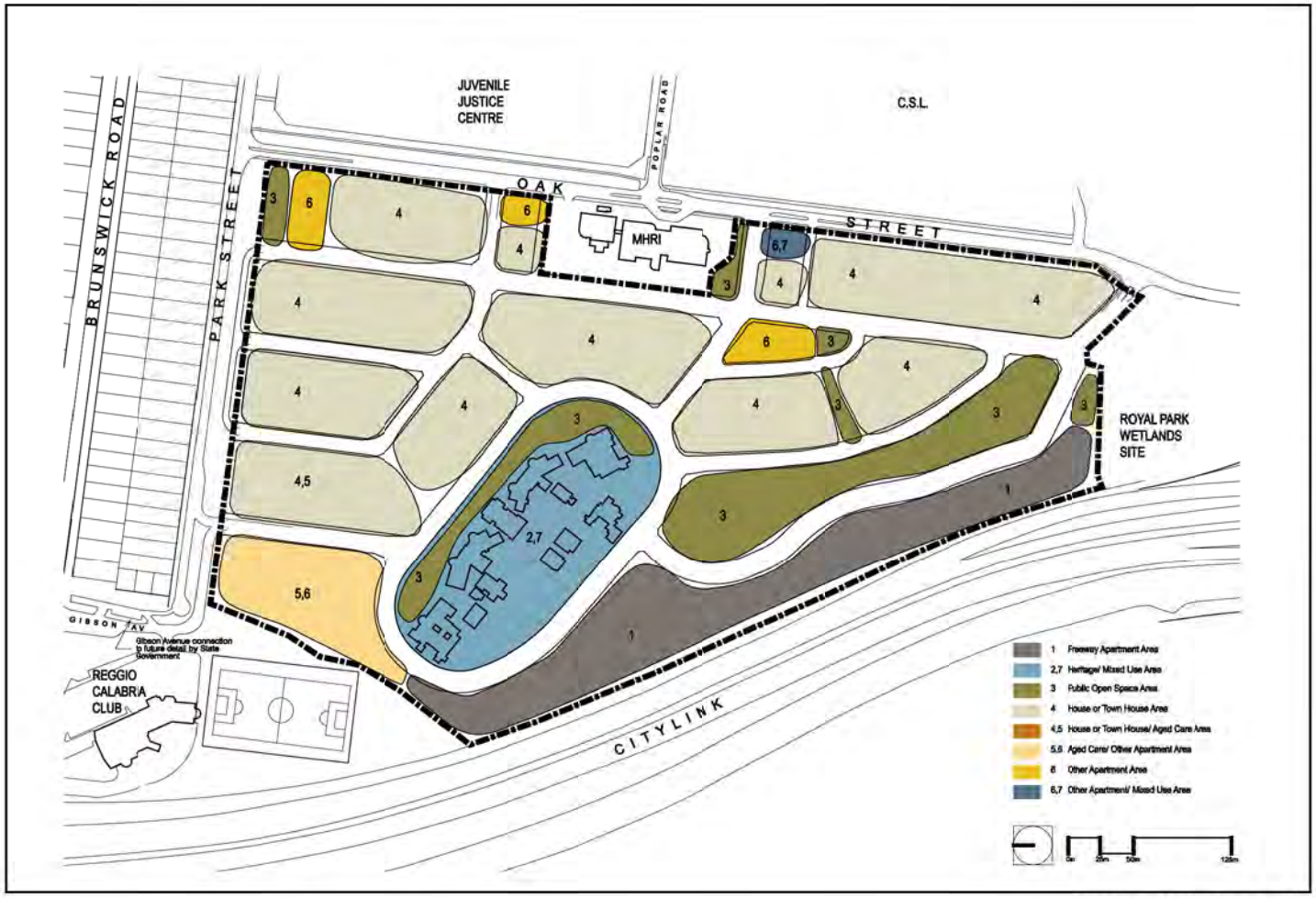
Retained trees together with extensive new plantings in streetscapes, open space reserves, and on allotments will result in an overall net gain in the number of trees in the Village between the start and final completion of the project.

Tree protection zones will be established to protect Category 1 trees from the impact of construction. The area differs in size according to the drip zone and size of individual trees. TreeLogic's Arboricultural Assessment and Report (Nov 2003) as attached includes detailed tree protection guidelines that will be followed. Pruning and root preparation shall also be carried out on the basis of an Arboricultural assessment.

TREE REMOVAL PLAN


OAK STREET
PARK STREET
CITYLINK
LEGEND
TREE PROTECTION ZONE PLAN

1.03 SITE LAYOUT PLAN

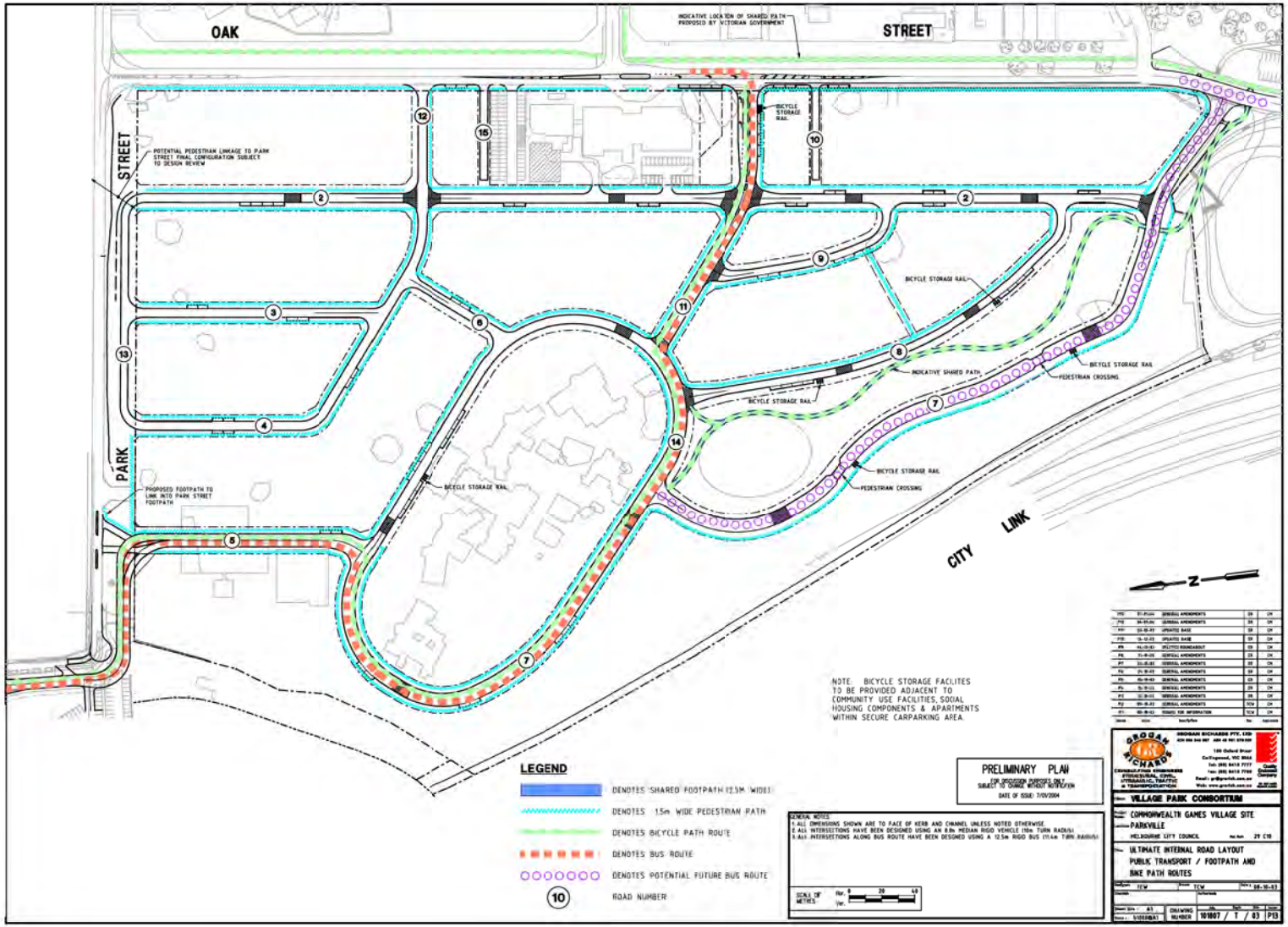


SITE LAYOUT PLAN

The site layout plan is generally in accordance with the indicative site layout plan in clause 15 of the incorporated document. The plan provides for at least 900 dwellings comprising a modulated line of freeway apartments on the western boundary and a mix of houses, townhouses and other apartments elsewhere on the site.

The site layout plan adopts a primary north-south road network layout that, in addition to the built form, landscape treatment and interface conditions along Oak Street, Park Street and CityLink, promotes the integration of the site with surrounding area, particularly the residential neighbourhood to the north. The proposal includes footpaths, cycling paths and lanes, public transport routes and facilities that are located to encourage non-car transport, and create a safe and accessible environment.

The proposed road/superlot layout facilitates the use of the land for the Commonwealth Games Village and the associated functional zones. The linear park separates the residential area to the east from areas prescribed for shared athlete facilities (including the dining hall and transport mall) and village operations and support areas. The Clinical Services Buildings and heritage buildings (or part thereof) will be utilised for administrative functions. The proposed roads will provide access for athletes and visitors to this facility and to the Village generally.



Bus, Bicycle & Pedestrian Networks

Bus Route

It is proposed that the existing bus service (route #504) operating along Brunswick Road travelling from Moonee Ponds Junction to Clifton Hill Station via North Carlton, will be diverted through the site. The exact route through the site is subject to detailed design and negotiations with bus operators. However, an optional route includes diversion from Brunswick Road via Fleming Street, Park Street, Oak Street, through the site, then returning to Brunswick Road at the Gibson Avenue intersection.

The proposed bus route through the site is shown on the attached plan. A road pavement width of 8 metres and semi indented bus bays will be provided on the bus route. The proposed central location of the bus stops adjacent to the heritage precinct will permit good access for residents of the Village especially those from the aged care facility, freeway apartments and heritage precinct.

Bicycle Path

Bicycle path routes have been designated through the site. The bicycle path routes through the site will be provided in 2 ways:

- Off road – as a shared 2.5 metre wide footpath adjacent to the parkland.
- On road – as a 1.0 metre wide bicycle lane in each direction within an 8 metre wide pavement, extending from the Park Street entrance to Oak Street.

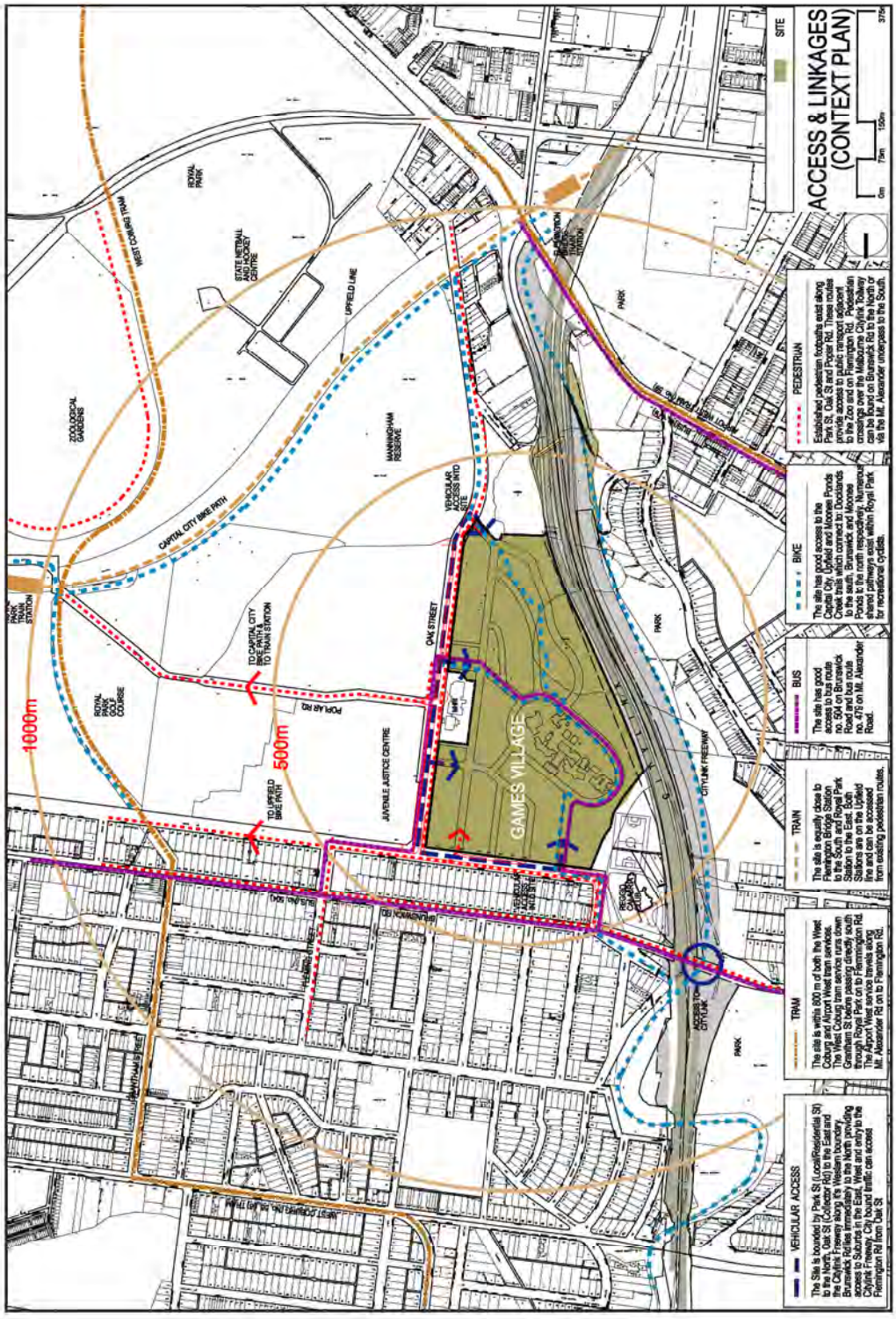
The bicycle route will connect with a regional bicycle system being developed by the state government and provide a safe and integrated network.

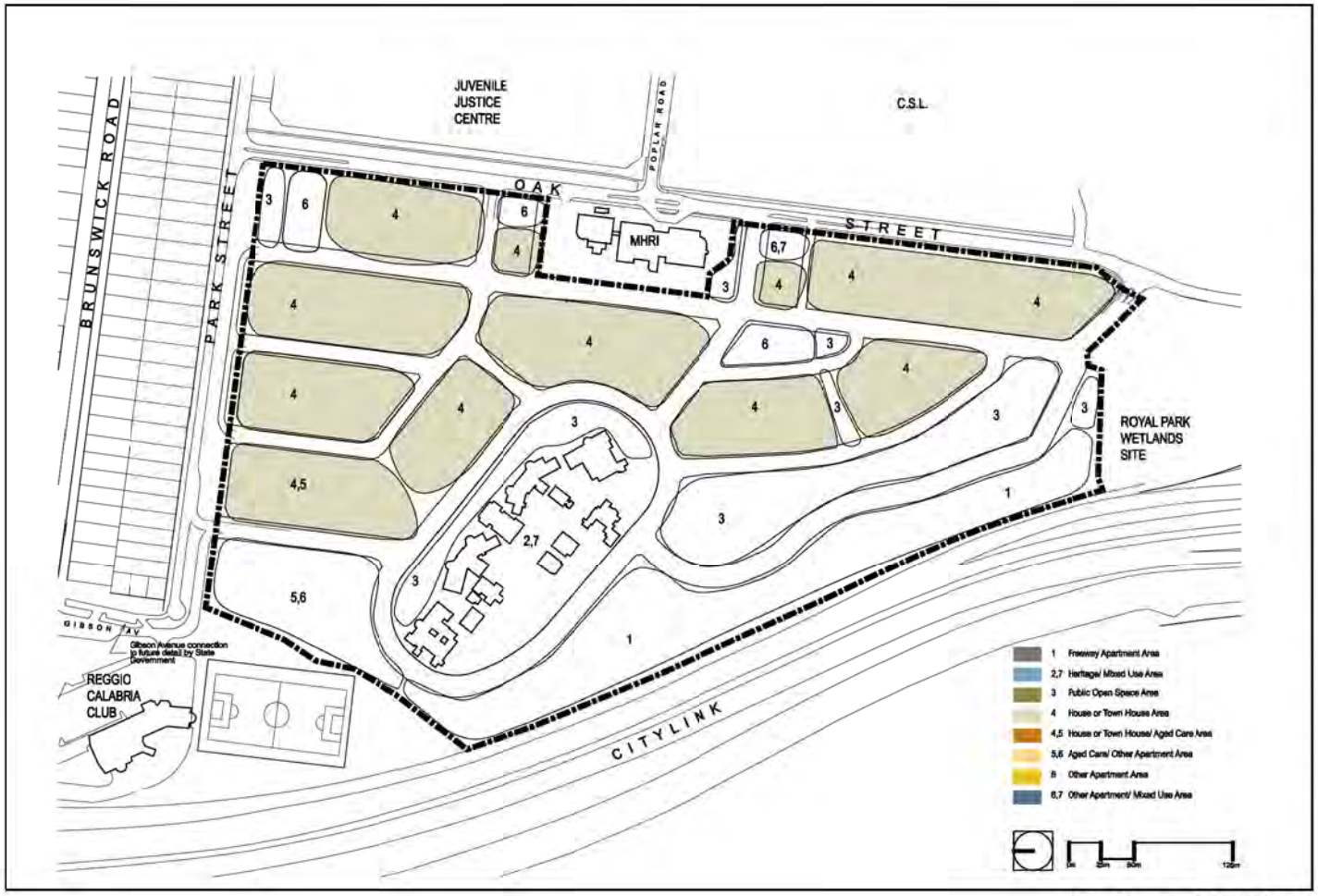
Pedestrian

Concrete footpaths 1.5m wide will be provided to the front boundaries of all properties as generally indicated on the attached plan. Footpaths shall be designed to provide access for people with disabilities to all houses, townhouses, apartments and public open space throughout the Village.

Footpaths may be excluded on streets (or part thereof) that are not fronted by dwellings or form the edge to public open spaces, in order to increase the total green space and provide opportunities for varied landscape treatments.

Footpaths will be integrated with the shared pathway network and existing footpaths (where provided) on Oak Street and Park Street.


ACCESS & LINKAGES
(CONTEXT PLAN)
SITE
PEDESTRIAN
BIKE
BUS
TRAIN
TRAM
VEHICULAR ACCESS
GAMES VILLAGE
1000m
500m
CAPITAL CITY BIKE PATH
UPFIELD BIKE PATH
TO CAPITAL CITY BIKE PATH & UPFIELD STATION
OAK STREET
POPLAR RD
JUVENILE JUSTICE CENTRE
ROYAL PARK GOLF COURSE
ZOOLOGICAL GARDENS
ROYAL PARK
STATE NETBALL AND HOCKEY CENTRE
WEST COBURG TRAM
UPFIELD LINE
MANNINGHAM RESERVE
PARK
CITYLINK FREEWAY
ACCESS TO CITYLINK
PARK PARK STATION



HOUSING PRECINCT

Housing Precinct (4)

The house and townhouse precinct shall generally comprise of approximately:

- 165-170, four bedroom houses;
- 7 four bedroom social housing townhouses; and
- 6 two bedroom social housing apartments.

Development in this precinct will not exceed three storeys.

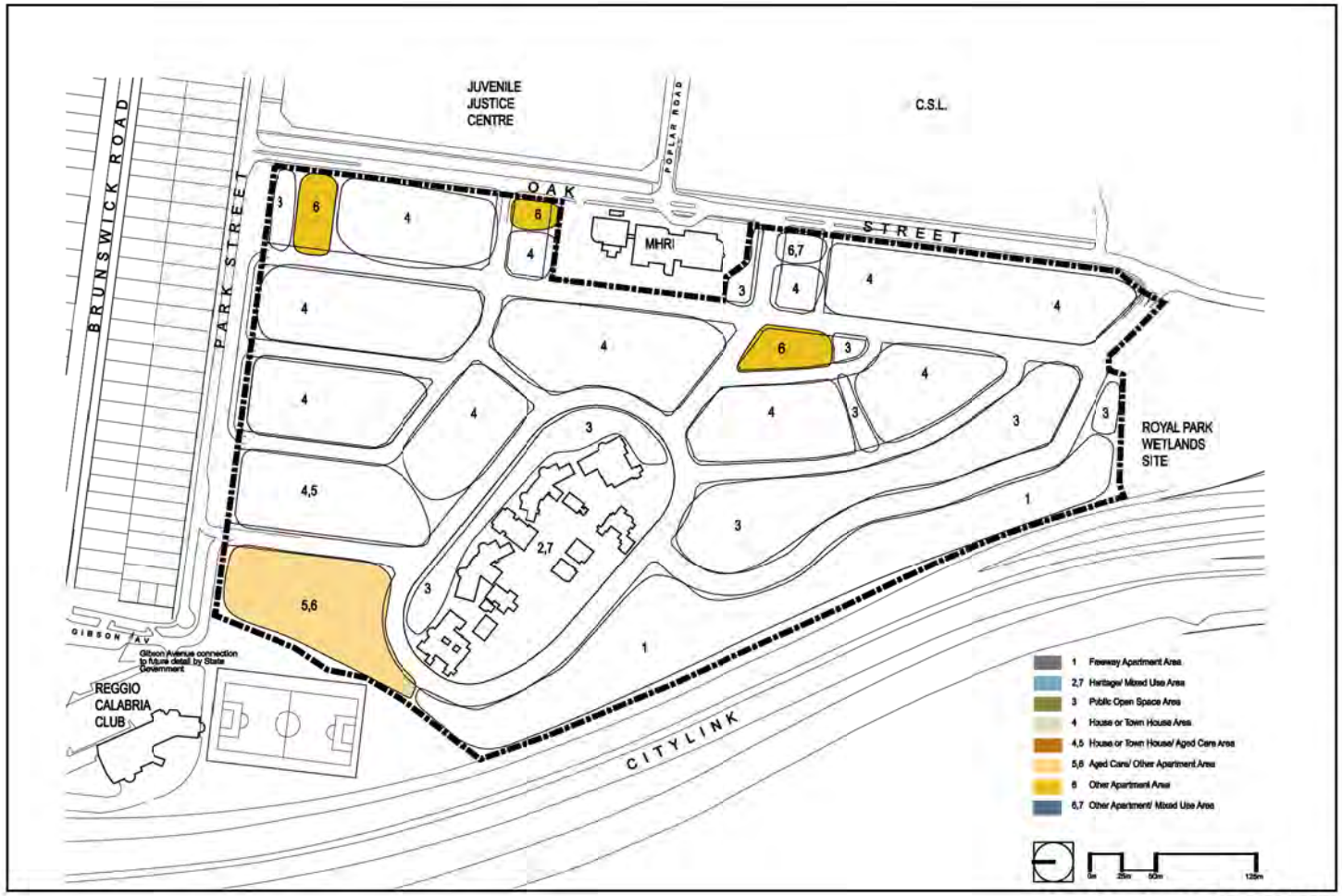
The proposed townhouses and apartments (detailed above) are to be co-located on the parcel of land to the north of, and abutting, the Mental Heath Research Institute.

Future subdivision of the land within the housing precinct will orientate lots and dwellings to take advantage of solar access. Houses, townhouses and apartments will be designed to protect the amenity of adjacent residential uses with respect to overlooking, privacy, access to sunlight, access to daylight for habitable room windows, useable private open space, visual bulk and opportunities for solar collection and passive solar access.

Disabled access will be provided to all houses, townhouses and apartments (where lifted) where permitted by the existing topography of the site. In addition, at least 20% of housing will have internal layouts that can be modified or adapted at a later date to enable wheelchair access.

Dwellings constructed will be consistent with the principles of sustainable development. This will include the aim to achieve a six star energy efficiency rating to all houses, (as rated by First Rate Software) and the installation of gas boosted solar hot water systems that will aim to achieve a minimum 60% solar contribution to the hot water system.

1.05 LAND USE PLAN: APARTMENT ZONE



APARTMENT PRECINCT

Apartments Generally (Precincts 1 and 6)

Apartments in the Village will achieve an average apartment height of approximately six habitable storeys[1] across the site.

The design of apartment buildings will respond to their context taking into account sightlines external to the site. They will make a positive contribution to the built form of the area, provide architecturally diverse and interesting facades, appropriately integrate roof-mounted structures and services, and provide a high level of internal amenity for residents. Apartments allocated for social housing shall be designed and constructed to be visually integrated with the surrounding development.

All apartments shall achieve the minimum energy efficiency rating prescribed at the time of construction, and to aim to achieve an overall energy efficiency rating for the apartments higher than the minimum prescribed. Disabled access shall be provided to all lifted apartments.

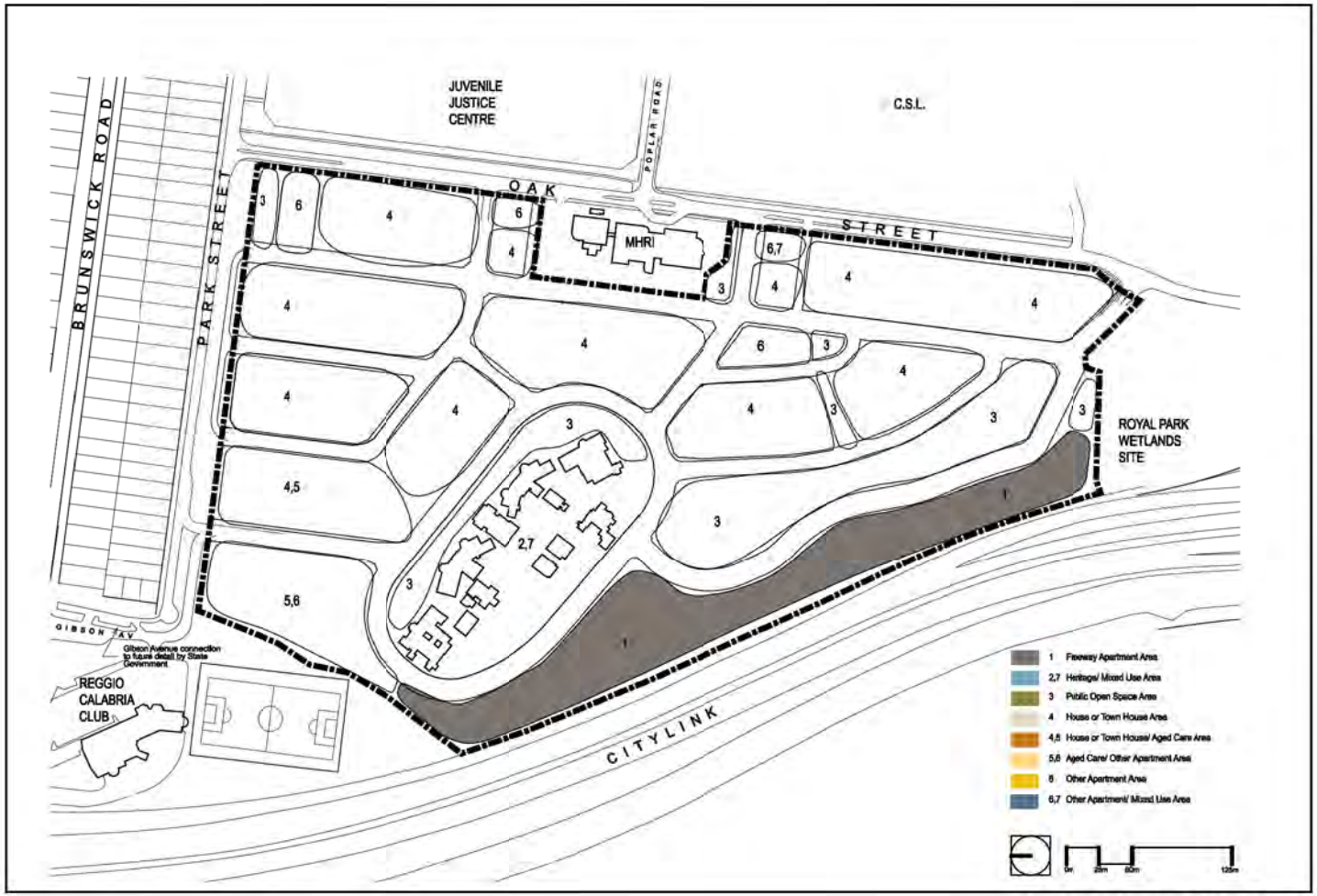
Other Apartments (Precinct 6)

A social housing apartment building is proposed for the corner of Park and Oak Streets. This building shall include 30 elderly persons units over three habitable storeys and associated carparking. The exact siting will be determined taking into account the landscaping, topography and access issues.

A market apartment building is proposed for the land west of, and adjacent to, the Mixed-Use Precinct. This building shall not exceed 4 habitable storeys.

The design of both buildings shall achieve the objectives described for apartments generally (above).

Note 1: Habitable storeys excludes carparking levels.



APARTMENT PRECINCT

Apartment (freeway) precinct (Precinct 1)

The apartment zone will include a modulated chain of approximately 700 apartments ranging in height from three habitable storeys up to, but not exceeding, ~~eleven~~ nineteen habitable storeys. The apartment buildings adjacent to the retained heritage buildings will be restricted to a maximum of five habitable storeys in height and be designed is such a way as to respect the integrity and significance of the heritage buildings.

The layout of the buildings along the western boundary of the site shall be designed such that they provide an effective acoustic barrier for the balance of the land and enhance and respect the integrity of the existing Melbourne Gateway.

Three metres of land shall be reserved between the apartment building and the noise wall along City Link to provide access between the wall and the apartment buildings. Generally, access to the apartment buildings will be adequate and appropriate for service, emergency and delivery vehicles and parking will be adequate for tradesmen and removalist vans.

At the southern end of the apartment precinct VPC shall develop a high quality apartment building that aims to provide passive surveillance over the wetlands and pedestrian routes and form a visual and physical link between the open space network within the Games Village Project to both the wetlands and Royal Park.

Parkville Gardens Freeway Apartments
Siting and Design Guidelines
Amended December 1, 2016

FRASERS PROPERTY | citta property group

Project Number: 21157
Date: Dece,ber 1st, 2016
Client: Village Park Consortium [VPC]

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Contents

Introduction

The residential development of Parkville Gardens, the name given to the 2006 Commonwealth Games Athletes Village site, is a three stage development comprising:

Games mode development (completed December, 2005)
Post Games retrofit (April 2006 – January 2007)
Post Games development (post 2006-~~2020~~ 2018)

The post-Games development comprises the balance of works to bring the Village to final completion. These works include:

- the construction of 13 no. houses for private sale
- the development of Lot 8
- the construction of an Aged Care Facility
- the development of at least 1300 no. residential apartments for private sale in a modulated chain of buildings constructed along the western boundary of the site (Freeway Apartments precinct), including at least 18 no. for use by the Office of Housing.

This document proposes revised "Siting and Design Guidelines" for the above development in the Freeway Apartments precinct as part of the approved Village Masterplan.

Planning Framework

Aerial Site Photo

The subject land is affected by site specific planning controls, pursuant to Clause 52.03 of the Melbourne Planning Scheme. The site specific controls are contained in a document incorporated in the planning scheme and entitled "The Games Village Project, Parkville, 17 October 2003".

Pursuant to Clause 5 of the Incorporated Document, the subdivision, use and development of the land for any purpose associated with the Games Village does not require a planning permit, provided it is in accordance with the Incorporated Document.

Before the Minister approves development plans for any stage of development, a Master Plan must be submitted to and approved by the Minister in accordance with Clause 7 of the Incorporated Document. This has been carried out.

Clause 8 requires a series of Integrated Plans to be approved by the Minister prior to the approval of development plans. Each of these plans has also been prepared and approved by the Minister.

Pursuant to Clause 9, Siting and Design Guidelines are required to be approved for each precinct of the development, prior to the approval of development plans.

The Siting and Design Guidelines must address, as appropriate for each precinct:

- relevant design objectives for built form in each precinct, including height, setbacks, massing and roof form
- materials and finishes
- design and integration of building services including environmental features
- acoustic performance in habitable rooms
- accessibility requirements
- views from key vantage points external to the site including the Citylink, Travancore, Royal Park, Oak Street and Park Street
- the applicable design objectives in Clause 10 of the Incorporated Document

Siting and Design Guidelines have been prepared and submitted to the Minister for approval for the houses and townhouses precinct, the other apartments precincts (excluding Development Lot 8), the aged care precinct and the mixed use precinct. Construction of the houses and townhouses, Heritage precinct, other apartments and the aged care precinct are completed.

The Siting and Design Guidelines for the Freeway Apartments precinct need to respond to the design objectives outlined in Clause 10 of the Incorporated Document as follows:

Heritage Precinct buildings adjacent to Subject Site

Cl 10.1 For the overall development

- To provide at least 20% of housing with internal layouts that can be modified or adapted at a later date to enable wheelchair access.
- To ensure that dwellings allocated for social housing are designed and constructed to be visually integrated with the surrounding development.
- To design all dwellings to meet the appropriate noise standards
- To protect the amenity of adjacent residential uses with respect to overlooking, privacy, access to sunlight, access to daylight for habitable room windows, useable private open space, visual bulk and opportunities for solar collection and passive solar access.
- To provide footpaths that are at least 1.5 metres wide.
- To provide disabled access to all houses, townhouses and apartments (where lifted).
- To incorporate water sensitive design techniques into the design of roads, landscaping, public open space and other developments having regard to the integrated water management plan and stormwater management systems for the site.
- To protect the structural integrity and access to the Royal Park Main Drain, the Moonee Ponds Sewer Main and the Moonee Ponds Sewer Main Deviation.
- To avoid uniform development and encourage diversity of design.
- To provide high quality residential development including integrated social housing, community facilities and public open space.
- To apply the principles of sustainable development.
- To encourage the use of public transport.
- To provide appropriate built form, landscape treatment and interface conditions along Oak Street, Park Street and Citylink and with other uses that adjoin the Land.
- To implement the integrated plans approved under Clause 8 of this Incorporated Document.

Recently completed buildings adjacent to Subject Site

Cl 10.3 For apartments generally

- To design apartment buildings that respond to their context taking into account sightlines external to the site.
- To provide appropriate access to the apartments for service, emergency and delivery vehicles and adequate parking for tradespeople and removalists.
- To design all apartments to achieve the minimum energy efficiency rating prescribed at the time of construction, and to aim to achieve an overall energy efficiency rating for the apartments higher than the minimum prescribed.
- To promote high quality apartment developments that make a positive contribution to the built form of the area, provide architecturally interesting facades and provide a high level of internal amenity for residents.
- To conceal external plumbing pipes and fixtures, excluding downpipes.
- To integrate roof-mounted structures into the design of the buildings.
- To provide a diversity of architectural expression in the design of the apartment buildings.
- To provide a clearly identified pedestrian entry at street level.
- To provide adequate, safe and efficient car parking for residents and visitors. Car parking for residents should not dominate the streetscape.
- To provide bicycle storage facilities for residents and visitors in accordance with the rates set out in the parking precinct plan under Clause 8.8 of this Incorporated Document.
- To achieve an average apartment height of approximately six habitable storeys.

International Gateway Sculpture adjacent to the Freeway

Cl 10.4 For the Freeway Apartments

- To create an apartment layout along the western boundary of the site that is dynamic, articulated, curvilinear and highly modulated and enhances the existing Melbourne Gateway.
- To create a modulated layout from three habitable storeys up to, but not exceeding, ~~eleven~~ nineteen habitable storeys in height with the tallest elements representing urban markers for the Games Village Project.
- To restrict the apartment buildings adjacent to the retained heritage buildings within the Heritage precinct to a maximum of five habitable storeys in height.
- To develop a high quality apartment building at the southern end of the Freeway Apartments precinct that aims to provide:
  - An acoustic buffer
  - Passive surveillance over the wetlands and pedestrian routes
  - An urban marker; and
  - A visual and physical link between the open space network within the Games Village Project to both the wetlands and Royal Park.
- To ensure that the façade of the apartments facing the western boundary of the site is of a high architectural standard and does not include any advertising.
- To minimise any increase in traffic noise in Travancore through the design and use of materials on the western façade.
- To respect the integrity and significance of the retained heritage buildings.
- To reserve ~~three~~ five metres of land between the apartment building and the noise wall along Citylink to provide access between the wall and the apartment buildings.
- To provide an effective acoustic barrier for the balance of the Land.
- To respect the architectural integrity of the International Gateway Sculpture along the Citylink Freeway.

Parkville Gardens precincts

	1	Freeway Apartments
	2,7	Heritage/ Mixed Use
	3	Public Open Space
	4	House & Town House
	4,5	House & Town House/ Aged Care
	5,6	Aged Care/ Other Apartments
	6	Other Apartments
	7	Mixed Use

Site Location Plan

The Site

Freeway Apartments precinct

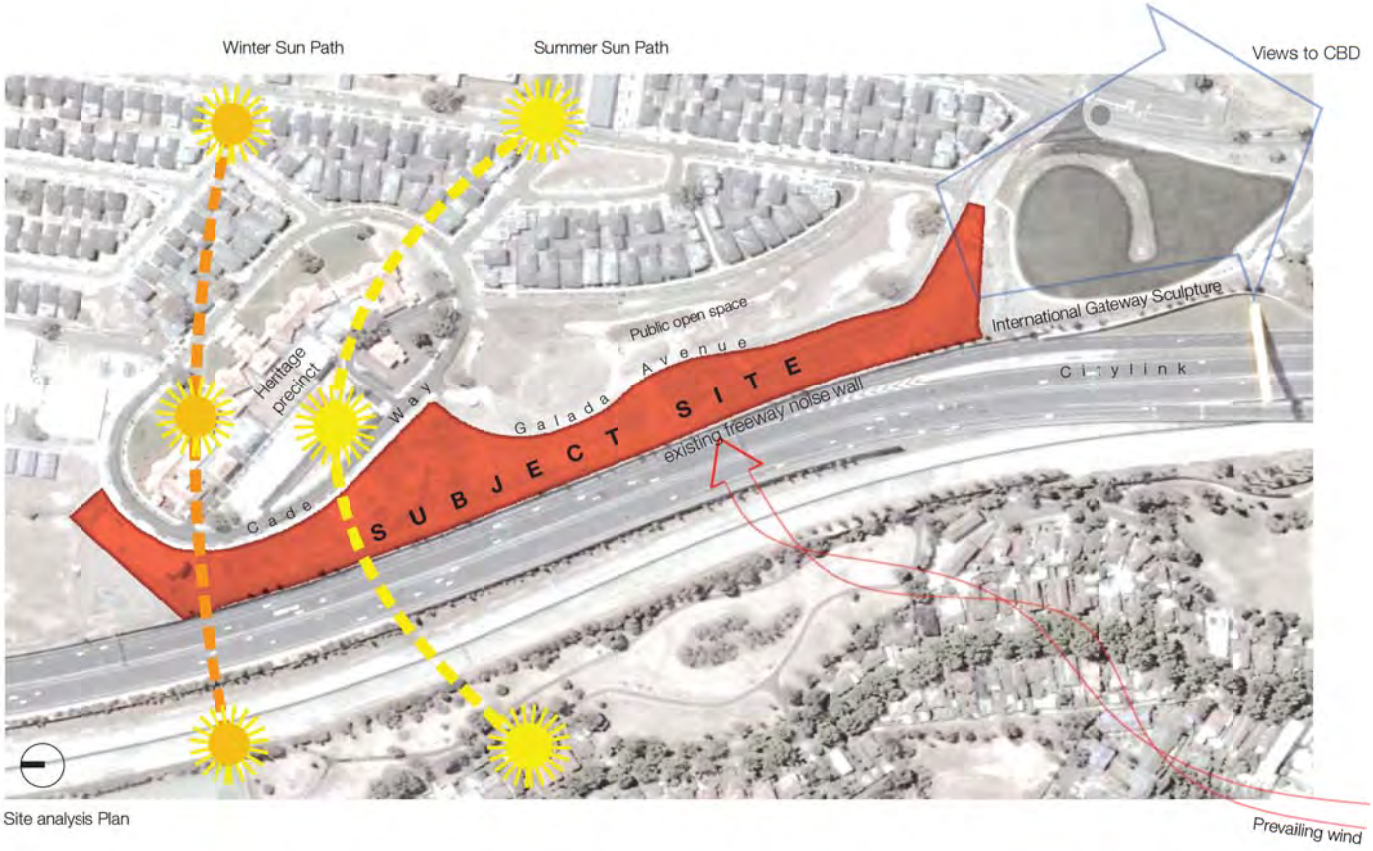
The indicative site layout plan in Clause 15 of the ncorporated Document defines various precincts within the overall Parkville Gardens site. The Freeway Apartments precinct (no.1) is characterised by the following:

- The site is bounded to the north-east by Cade Way, which encircles the Heritage precinct.

The site is bounded to the east by Galada Avenue. Construction of Galada Avenue has ~~not~~ commenced and these works will be staged to suit the construction of the Freeway Apartments. Services for the Freeway Apartments will be drawn from infrastructure provided in Galada Avenue. The alignment and levels for Galada Avenue have been fixed.

- The site is bounded to the west by the Citylink noise-wall. The wall varies in height between 4.8 and 6m. It is constructed from precast panels set between steel columns. The site boundary for Parkville Gardens approximately follows the wall alignment with an offset into the site of approximately 0.5m. The noise-wall is owned and maintained by Citylink. There are some in-ground communications services belonging to Citylink running beside the wall.
- A 5m wide access road will be provided along the inside face of the freeway wall in accordance with Section 11 of the Minister's letter of January 2004 approving the Master Plan.
- There are a number of small and medium sized trees growing beside the freeway wall. These are not identified in Section 1.02 'Tree Retention Plan' of the approved Master Plan and therefore do not need to be preserved.
- A social housing apartment building, called Precinct 11, has been constructed at the northern tip of ~~the Site~~ Precinct 1 (Not shown on the adjacent plan). This building includes 20 apartments over 4 levels. Carparking is provided in an under-croft carpark.
- Precinct 1 Freeway Apartments, Stages 1-3 and 5-6 have been constructed and consent approval has been issued for Stages 4,7&8. This revision seeks to amend the guidelines for Stages 9-12 and reflect the "as built" and approved Stages 1-8. Refer Pages 18 + 20 for Stage locations.
- The site is generally clear and open.

The Parkville Gardens site has been reticulated with servicing infrastructure including sewer, water, power, communications, natural gas and stormwater drainage. This infrastructure will be utilised to service the Freeway Apartments precinct in accordance with the associated regulatory authority requirements. An existing 975mm trunk sewer main traverses the site at the southern end of the precinct. Melbourne Water has advised that this live sewer main ~~may~~ cannot be diverted around the Freeway Apartments building zone ~~or could be~~,and is to be left in its current alignment with the creation of an ~~6~~ 11 metre non-building zone easement over the pipeline. ~~Further investigation into the most viable option for the sewer will be undertaken during design of the associated stage of the Freeway Apartments development.~~



Site analysis Plan

Site analysis

Existing solar access

- This diagram illustrates the tracking of the sun in winter and summer. The site has excellent solar access due to the site's large open configuration.

Adjacent context

- The site is bounded by three primary interfaces. The western boundary is described by the existing freeway noise wall. The eastern boundary interface is comprised of the public open space along Galada Avenue and the Heritage interface along Cade Way which has a number of buildings that range from 1 to 4 storeys in height.
- There are no habitable room windows or private open space within 9 ~~meters~~ metres of the site.

Overshadowing/overviewing

- Due to the large open nature of the site and the substantial lack of adjoining private property to the site, No overshadowing and/or overviewing issues exist.

Access and egress

- The site is bordered by Cade Way along the Heritage precinct interface and Galada Avenue along the linear Park Interface.

Internal/external views

- The site has external views to the International Gateway Sculpture and the Melbourne CBD to the south.
- The site has extensive boundary interfaces that address the Citylink Freeway to the west and the Heritage precinct and the public open space to the east.

Significant vegetation

- Within the boundaries of the Freeway Apartments precinct, the approved masterplan has not identified any Category 1 or 2 trees to be retained.

Existing buildings



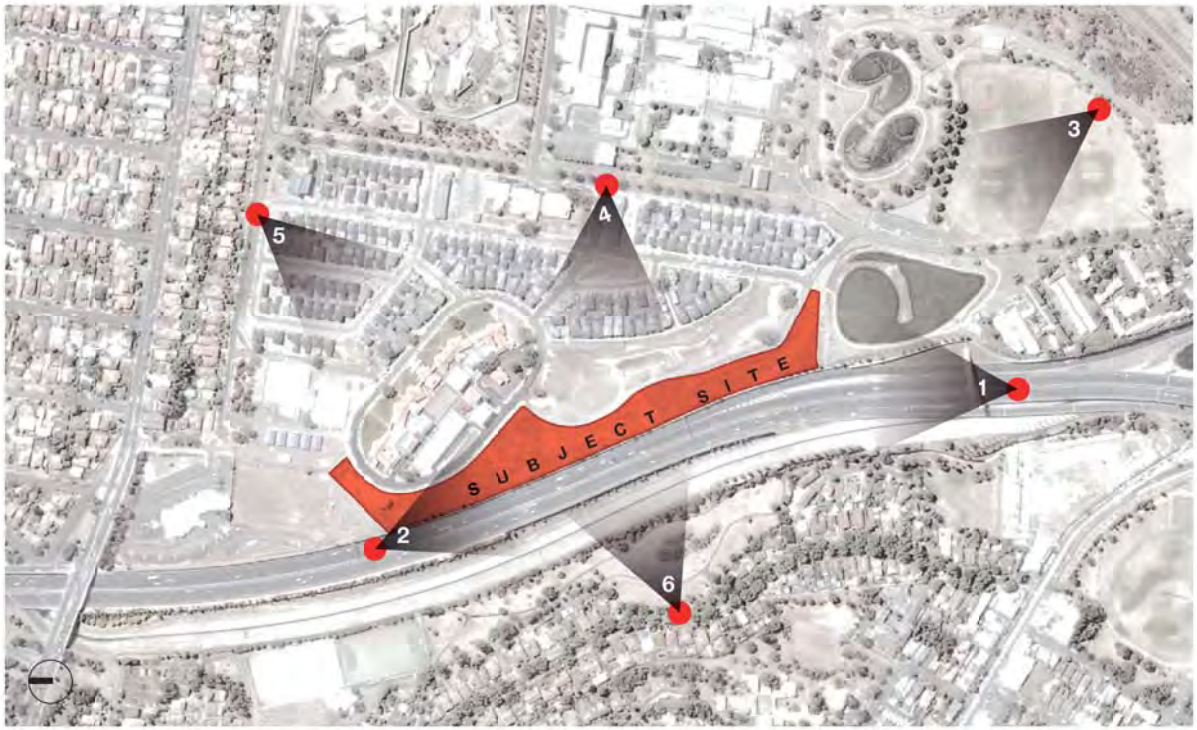



Recently completed residential buildings adjacent to the Subject Site

A four level social housing apartment building is constructed adjacent to the northern end of the Freeway Apartments precinct. In the adjoining Heritage precinct, single storey, period residences are located to the east of new townhouses, home-offices and social housing apartments which range in height from 2 to 4 levels.

~~Several of these new buildings align with Cade Way opposite the Freeway Apartments precinct.~~

Within the Freeway Apartment Precinct buildings ranging between 2 & 4 storeys have been constructed within Stages 1 to 3.
Stage 4 has been approved at 15 storeys (11 habitable)
Stages 5+6 have been constructed rising to 8 storeys (8 habitable)
Stage 7 has commenced construction at 15 storeys (11 habitable)
Stage 8 has been approved at 13 storeys (9 habitable)



Site Plan- Key view locations

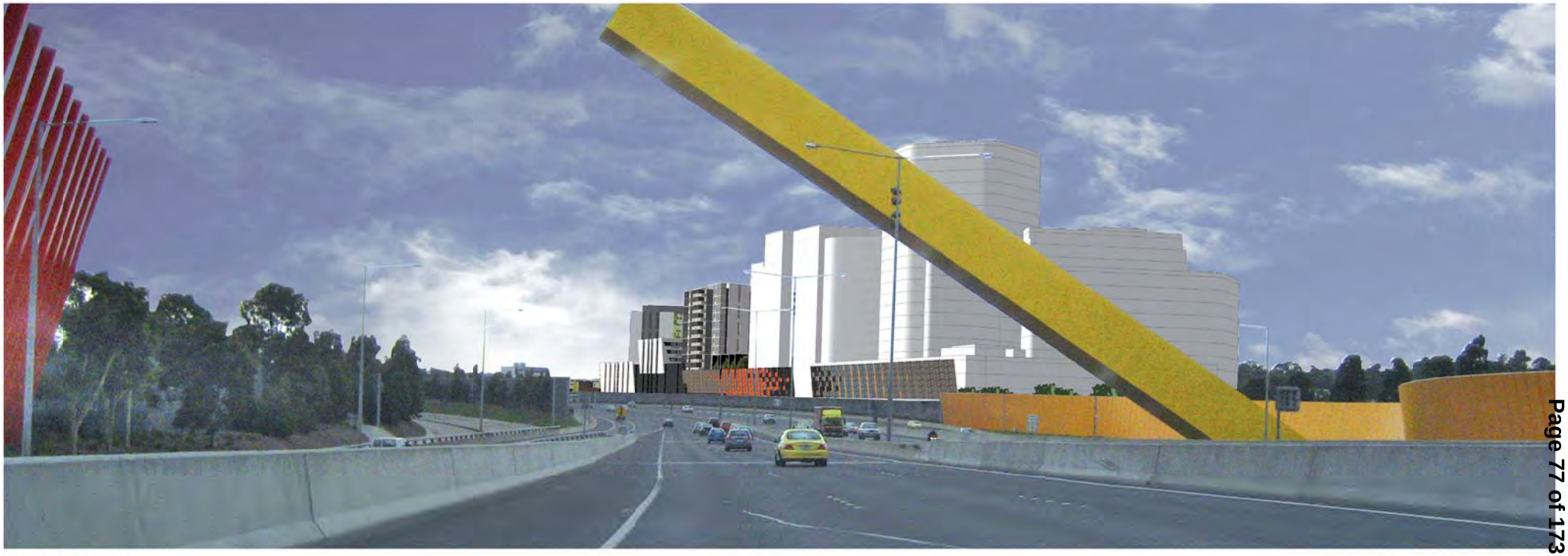
Key views

The Incorporated Document refers to specific key vantage points external to the Freeway Apartments precinct. These are identified as follows:

1. Citylink outbound
2. Citylink inbound
3. Royal Park
4. Oak Street
5. Park Street
6. Travancore Park

In assessing how the proposed development should respond to these key vantage points, the following criteria are to be considered:

- Existing built form/landmarks
- Landscape/topography
- View Orientation



Citylink outbound

- Photographic analysis

Existing built form/landmarks

- The individual elements of the International Gateway Sculpture are clearly visible and frame the view along the Citylink Freeway.
- The existing noise wall obscures the low level view into the Freeway Apartments precinct.
- No important views are available to the existing built context in the background.

Landscape/topography

- The Citylink freeway slopes down toward the site from the freeway "noise tunnel" which offers upper level views.
- The foreground is characterised by the roadway, dividing barrier walls and landscaping to the west.

View orientation

- The full length of the site and the proposed apartment buildings are visible above the noise wall and existing vegetation.

- Desgn assessment

Form

- The proposed built form will provide a backdrop beyond the International Gateway Sculpture as a curvilinear ~~wall~~ building arrangement with articulated individual building elements . The verticality of the ~~proposed wall system~~ buildings will contrast with the angular form of the individual Sculpture elements. The expressed texture of the new ~~wall~~ articulated buildings will also provide a contrasting backdrop to the International Gateway Sculpture with the smooth, painted surfaces of the existing wall and 'stick' elements.

Materiality

- The use of precast concrete with a simple colour schemes will contrast with the vibrant primary colours of the International Gateway Sculpture, maintaining the prominence and clear identification of the original DCM design.

Citylink inbound

- Photographic analysis

  Existing built form/landmarks
  - Individual elements of the International Gateway Sculpture, tall buildings in Travancore and Central Activity District buildings are visible in the distance.
  - The noise walls to both sides of the freeway are dominant objects within the field of view from the freeway.

  Landscape/topography
  - The Citylink freeway remains relatively level with a gentle fall toward the east.

  View orientation
  - The views of the proposed built form are primarily oblique in nature due the proximity to the existing noise barrier wall and the parallel alignment of the Citylink freeway.
  - Close views of the proposed built form will not be seen in complete elevation due to the length and position of the proposal within the wider context.

- Design assessment

  Form
  - The varied height of the new ~~wall~~ buildings will contrast with the low level, even height of the freeway noise walls.

  Materiality
  - The use of precast, textured concrete with simple colour schemes will contrast with the vibrant primary colours of the International Gateway Sculpture and the consistent colour treatment of the noise walls.

Royal Park

- Photographic analysis

Existing Built Form/Landmarks
- The International Gateway Sculpture is visible beyond the suburban landscape/wetlands interface
- Level parkland and sporting ovals are visible in the foreground with the freeway noise wall in the distance

Landscape/Topography
- The higher ground of Travancore Park is visible beyond the Freeway
- This view is characterised by the interface between parkland and suburban housing

View Orientation
- An oblique view of the Freeway Apartments precinct is available with existing vegetation obscuring parts of the proposed built form

- Desgn assessment

Form
- The proposed landmark building to the south of the Freeway Apartments precinct will be clearly identifiable with lower modulated buildings visible running north-south along the site.

Materiality
- A variety of materials and surfaces will be incorporated, specific to the orientation of components & position of buildings within the precinct.
- Lightweight, glazed facades ~~to the east facing spine apartments contrast~~ with modulated precast panels ~~to the north and south faces of the tower buildings~~. Low level, street-edge apartments utilise a combination of timber and masonry for the facades facing the street and the landscaped podiums.

Oak Street

- Photographic analysis

  Existing Built Form/Landmarks
  - No distant and/or landmark views available
  - Primary views toward individual houses and the street network inside Parkville Gardens

  Landscape/Topography
  - Flat terrain incorporating private open space, driveways, urban landscaped areas etc.

  View Orientation
  - This view predominately reveals the residential nature of the adjoining precincts

- Design assessment

  Form
  - The proposed built form will be partly revealed in the distance, above the roof line of existing houses. The visual impact of the proposed buildings will be reduced by separation between towers and articulated facade treatments.

Park Street

- Photographic analysis

  Existing Built Form/Landmarks
  - Distant views of International Gateway Sculpture elements and Travancore Park are available above the suburban roofscape

  Landscape/Topography
  - Limited views from Park Street towards the Freeway Apartments precinct vary depending on the vantage point and the incline of the street

  Vew Orientation
  - The ~~proposed~~ Aged Care facility will prevent views towards the proposed built form at the west end of Park Street

- Design assessment

  Form
  - Overall, the proposed built form will have little impact on existing views from properties adjoining Park Street. However, intermittent views of the International Gateway Sculpture from the elevated, east end of Park Street will be obscured by the proposed development.

Travancore Park

- Photographic analysis

Existing Built Form/Landmarks
- Partial view of international Gateway Sculpture and Central Activity District are available in the distance.
- Intermittent views of housing to the east of the freeway are available beyond existing vegetation.

Landscape/Topography
- Contoured land form with extensive vegetation and open parkland

View Orientation
- The full view of the length of the Freeway Apartments precinct is available from this high level above existing vegetation in the foreground.

- Design assessment

Form
- The proposed built form will be visible above the existing vegetation within the park and it will not obscure any significant views.
- The stepped and curvilinear ~~form of the proposed wall~~ arrangement of buildings will be evident from this vantage point.

Materiality/colour
- A precast panel system is envisioned as the primary built element ~~of the wall~~ facing Travancore Park. The texture of the panels along the freeway will contrast with the soft nature of the park in the foreground. A simple colour scheme is proposed ~~for the wall~~ to form a backdrop to and compliment the International Gateway Sculpture.

Masterplan - Freeway Apartments precinct

Urban Design Objectives

Precinct masterplan

The intent of the Siting and Design Guidelines for the Freeway Apartments precinct is to develop a "diversity of place", based on the understanding that each precinct contributes to the quality of the entire Parkville Gardens site.

In particular, precincts are identified via their relationship to each other. The development within the Freeway Apartments precinct therefore needs to respond to the following:

- The low rise, residential character of buildings in the adjoining Heritage precinct encircled by Cade Way
- The public open space (linear park) to the east, adjacent to Galada Avenue
- The civic scale of Travancore, Citylink and the Melbourne Gateway.

The relevant design objectives for these three diverse edge conditions of the Freeway Apartments precinct are defined in the following pages.

A dynamic and diverse composition of built form is required to form a defined 'urban edge' to the site while still respecting and responding to the general residential neighbourhood character of Parkville Gardens. ~~Further analysis and a detailed application of the Siting and Design Guidelines could result in additional built form and/or apartment buildings compared with the proposed masterplan.~~

Existing period building to north east of the Subject Site

Interface between the Heritage Precinct & ~~recently~~ completed residential buildings adjacent to the Subject Ste

Perspective view indicating proposed interface with the Heritage precinct

Heritage precinct interface

Building envelope

The height of the apartment buildings fronting Cade Way and the adjoining Heritage precinct have been designed in accordance with the Indicative Building Heights nominated in Section 1.11 of the approved Master Plan and Clause 10.4 of the Incorporated Document.

Buildings in Stage 1 of the development at the northern end of the precinct have been restricted to a maximum height of 5 no. habitable storeys. This was to respect the integrity and significance of the pavilion-style heritage buildings which have been adaptively re-used for residences.

In the widest part of the site, near the corner of Cade Way and Galada Avenue, adjacent to the Heritage precinct interface, an apartment building of approximately 11 habitable storeys is ~~proposed~~ approved. This 'marker' building will contribute to the diversity of built form and provide a dialogue with the other taller building proposed at the south end of the precinct.

The Freeway Apartment buildings will generally be aligned with the freeway wall to the west of this precinct, with apartments orientated to create an active relationship to the Heritage precinct buildings. A series of apartment clusters front onto Cade Way. These street-edge apartments reinforce the sweeping boulevard effect of Cade Way, as well as providing a transition in height and density from the Heritage precinct to the Freeway Apartments precinct.

The existing four-storey, social housing apartment building located at the western extremity of the Heritage precinct has been integrated into the proposed development in the Freeway Apartments precinct.

Building layout

Building height

Building layout

- The open space created between tower buildings encourages passive surveillance, activity and occupation of the space.
- Tower buildings (S1 - S4) maintain separation ranging from 24m to 30m adjoining the Heritage precinct.

Building height

- Building heights correspond to the approved masterplan and Incorporated Document.
- At the interface with the existing built form in the Heritage precinct, building heights react dynamically to the site configuration/depth.
- Built forms range ~~up to~~ from 2 to 11 habitable storeys as measured at the ceiling of the top habitable floor.

Key	Building/Landmark	Australian Height Datum (AHD)
A	Social housing	25.4m
B	Social housing	26.0m
C	Citylink noise wall	14.2m
D	Townhouses	19.3m
E	Heritage building	19.5m

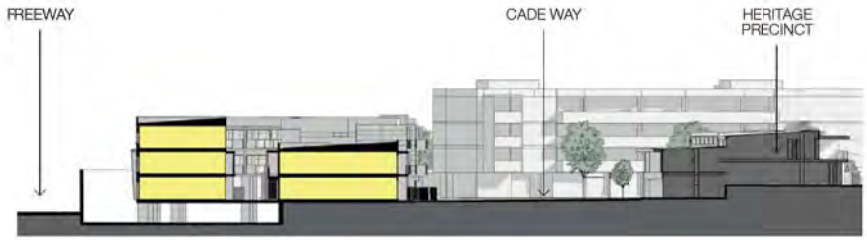
Building layout

Building setback

- ~~Proposed buildings~~ Building setbacks respond to the existing buildings across Cade Way.
- In areas where there is little or no existing built form directly adjacent to the Heritage Precinct, setbacks to the street ~~should~~ range from 0 - 4 metres (S1 and S2).
- Building setbacks have been formed by the curvilinear nature of the overall built form on the site.
- ~~Proposed buildings~~ Buildings opposite existing buildings in the Heritage precinct ~~should be~~ are lower in scale or set back from the boundary to respect the built context and open up space between.
- Where proposed buildings are opposite open space, setbacks can be minimised to the front boundary.
- The two storey carpark podium structure to the north of the site has been be built to the site boundary.

Project Sales Suite

- The project required a temporary Sales Suite for sales and marketing purposes.
- The single storey temporary Sales Suite was approximately 150sqm internally ~~and will include~~ with appropriate carparking located on the stage 4 site.
- The temporary Sales Suite ~~will initially be~~ is located on the site of ~~Tower No.~~ Stage 4. ~~The Suite will subsequently be moved to an appropriate location in the Public Open Space Interface zone.~~



Section AA

Indication of a 'green corridor' with passive surveillance from adjacent buildings

Perspective view indicating proposed interface with the Public open space precinct

Public open space interface

Building envelope

The location of buildings in the southern portion of the Freeway Apartments precinct provides apartments with a parkland outlook to the east of the site. This parkland is effectively a "green corridor" connecting Parkville Gardens to Royal Park. This landscaped buffer zone provides a significant setback from the established residential neighbourhood to the proposed Freeway Apartments.

Buildings in this area of the site create a layout that is dynamic, articulated, curvilinear and highly modulated, aligned with Galada Avenue.

This was designed to:

- Create a layout that is dynamic, articulated, curvilinear and highly modulated.
- Construct the "wall" of apartment buildings as a continuation from the Heritage precinct apartments, effectively creating a visual & acoustic buffer for Parkville Gardens from Citylink.
- Provide passive surveillance of the linear park and movement networks to the east of the Freeway Apartments precinct.
- Provide a diversity of design in the built form facing the linear park.
- Provide apartment buildings with a maximum height of ~~11~~ 19 habitable storeys.
- Provide varied setbacks to the street-edge ranging from a minimum of ~~2~~ 0 - 20 meters to avoid a uniform presentation to the street.
- Provide spacing between apartment towers ~~ranging from 10-20 metres~~ to vary the built form of the street-edge and creates visual interest.

Building layout

Building height (This elevation has been unfolded for clarity)

Building layout

- The open space created between tower buildings encourages passive surveillance, activity and occupation of the space.
- Tower buildings (S5 - S12) should maintain adequate separation ranging from 10 - 20 metres adjoining the public open space.

Building height

- Building heights are to correspond to the approved masterplan and Incorporated Document.
- Built form to range from 8 to ~~11~~ 19 habitable storeys as measured at the ceiling of the top habitable floor.
- Building should not unreasonably overshadow the adjacent wetlands (equinox 11am - 2pm)

Key	Building/Landmark	Australian Height Datum (AHD)
A	Melbourne Gateway [yellow stick]	45.4m
B	Melbourne Gateway [red sticks]	33.1m
C	Parkville Gardens typical residence	17.3m
D	Proposed Melbourne Water Sewer Easement	

Building layout

Building setback

- The propcsed building layout and setbacks from Galada Avenue respond to the width of the site by curving the position of individual buildings.
- The linear park opposite the site provides significant open space to which the apartments are orientated towards.
- The park provides visual separation between the existing houses to the east and the proposed built form in this part of the Freeway Apartmen:s precinct.
- At the south end of this precinct no building setback is proposed at the interface with the adjoining wetlands.
- An air and light easement will be placed along the public open space as required to the Stage 12 building form

Section BB

Perspective view indicating proposed interface with the Citylink Freeway precinct

Citylink/Melbourne Gateway interface

The western facade of buildings in the Freeway Apartments precinct should be a complimentary element to the Melbourne Gateway arrival experience. The contribution of this interface should provide a neutral backdrop to the sculptural qualities and significant presence of the International Gateway Sculpture elements.

The Gateway has been designed to be viewed from vehicles travelling along Citylink at high speed. This is the context within which the western facade of the Freeway Apartments will be viewed.

The design of the western facade should address the following key criteria:

- Individual apartment buildings should be clearly identifiable when travelling along Citylink.
- The selection of wall surface materials should enhance the appearance and legibility of the apartment buildings.
- The height of individual buildings is to be varied over the stages of the development to provide a layered profile to the precinct.
- Provision of a minimum 5m access road for maintenance between the Freeway Apartments precinct and the Citylink noise wall
- Provision of a generally continuous visual & acoustic buffer to the Freeway Apartments precinct and Parkville Gardens generally, as well as to Travancore.

Precinct	Apartment	Habitable Storeys	Apartment Length	No. Storeys x Length Weighting
6	Cnr Park & Oak	4	45	180
6	North MHRI	3	56	168
6	South MHRI	2	56	112
Heritage				
	Apartment Villas	1	230	230
		2	93	186
	Building 23	2	23	46
	Social Housing	3	20	60
Freeway apartment				
	Existing Social Housing	3	58	174
	Building 1 (Stage 1)	3	31	93
	Building 2 (Stage 1)	2	20	40
	Building 3 (Stage 1)	3	33	99
	Building 4 (Stage 1)	2	19	38
	Building 5 (Stage 2)	3	31	93
	Building 6 (Stage 2)	2	20	40
	Building 7 (Stage 2)	3	35	105
	Building 8 (Stage 3)	3	42	126
	Building 9 (Stage 3)	3	45	135
	Building 10 (Stage 4)	11	57	627
	Building 11 (Stage 5)	5	32	160
	Building 12 (Stage 6)	7	38	266
	Building 13 (Stage 7)	11	40	440
	Building 14 (Stage 8)	9	30	270
	Building 15 (Stage 9)	12	8	96
	Building 15 (Stage 9)	15	37	555
	Building 16 (Stage 10)	13	38	494
	Building 17 (Stage 11)	19	41	779
	Building 18 (Stage 12)	10	36	360
	Building 18 (Stage 12)	7	16	112
		Total	**1230**	**6084**
Average habitable storeys across Parkville Gardens			**4.95**	

Apartment height analysis

Under Clause 10 of the Incorporated Document the proposal should achieve an average height of approximately six habitable storeys.

- This schedule calculates the average number of habitable storeys for all apartments in Parkville Gardens. The schedule includes apartment buildings already completed in Precincts 1,3 and 4, Stages 1- 12 of the Freeway Apartment precinct as well as the Heritage precinct. ~~and the proposed Freeway Apartments precinct.~~

This analysis shows that the proposal conforms to the requirements stipulated in the Incorporated Document.

Site Progress

Built Form

Precinct objectives

The architectural design response for the Freeway Apartments precinct will accord with the specific requirements of Clause 10.4 of the Incorporated Document:

"To create a modulated layout from three habitable storeys up to, but not exceeding, ~~eleven~~ nineteen (19) habitable storeys in height with the tallest elements representing urban markers for the Games Village Project."

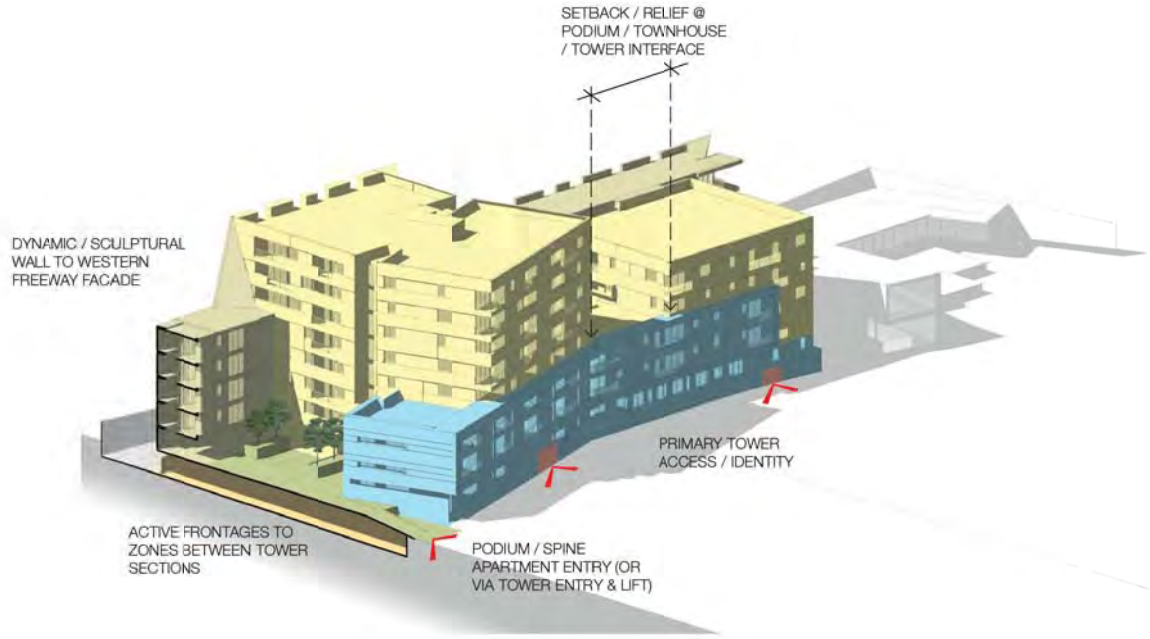
The form and layout of buildings which interface with apartments in the Heritage precinct should reinforce the strong 'circuit' form of this established precinct. The linear arrangement of apartment buildings fronting Galada Avenue and the Public Open Space (Linear Park) should contribute to a rhythmic composition, extending towards the southern end of the Freeway Apartments precinct.

To avoid uniform development and encourage diversity of design as required by section 10.1 of the Incorporated Document, the guidelines propose ~~four~~ distinct apartment typologies that serve to provide variation in accommodation, architectural expression and resident amenity.

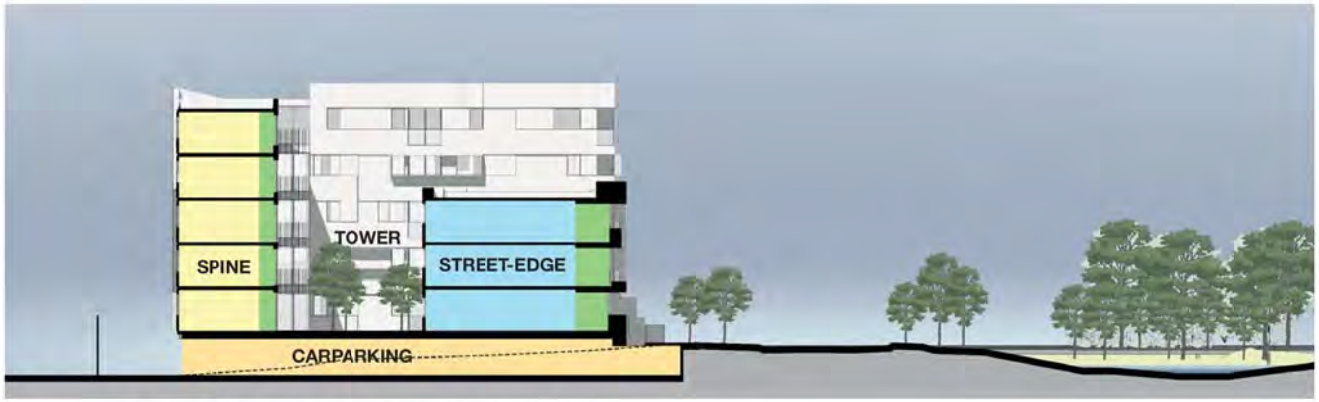
Planning Approved, Under Construction & Built

Proposed Future Stages

T# Tower Types (Refer to Following Pages)



Apartment Typology



East-west section through Freeway Apartments precinct (Stage 6)

Apartment building types

A diversity of design in the Freeway Apartments precinct will enhance the immediate locale of each apartment building while contributing to a constantly evolving experience of this significant site.
The building types or models created should respond to the specific constraints and opportunities of the Freeway Apartments precinct are as follows:

Tower type 01
In the Heritage Precinct Interface, these buildings will vary in height from ~~5~~ 3 habitable storeys at the northern end to ~~8~~ 5 habitable storeys in the southern end. In the Public Open Space Interface zone the buildings vary in height from ~~10~~ 9 - 11 habitable storeys. These provide passive surveillance to the park to the south. The tower apartments have been designed as a series of 'fingers' projecting towards the eastern parkland.

Tower type 02
The buildings have been designed in response to their proximity to the parkland to the east of the site. The form of the towers as individual elements emphasizes the residential nature of the buildings and creates a strong identity for the site. Height ranges between 5 - ~~11~~ 15 habitable storeys, the tallest at the southern end of the site adjacent to the parklands.

Tower type 03
This integrated cluster of three buildings forms the southern 'landmark' end to the Apartments Precinct. The distinct, curved and dynamic buildings are differentiated from other Stages and more 'typical' building forms, incorporating irregular, streamlined and sweeping forms as abstract, sculptural objects in the skyline.
The buildings are aligned around a radial arrangement of 'spines', creating a visual 'fanning' effect and a sense of layering, overlap and 'depth' in the arrangement and inter-relationships of buildings. The irregular building footprints and staggered, radial arrangement, coupled with effective internal building layouts, creates optimal potential for views outwards from the buildings, and access to daylight and sunlight.
The varied building heights provide variation in the scale of the buildings, and create a 'bookend' at the southern end of the Precinct.

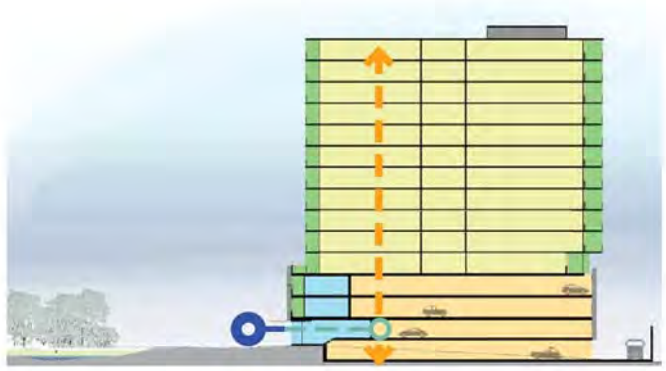
Street-edge / Podium apartments
These apartments will be orientated to the street, parkland and heritage precincts beyond, incorporating detailed elements and textured surfaces to provide a transitional architectural form between the street and the higher apartment towers beyond. A strong connection to the street will be established with terraces/balconies and glazed living spaces, creating an identifiable residential interface and enhancing passive surveillance of the streets and park.

Spine apartments
The east-facing apartments effectively provide a continuous visual and acoustic buffer to the Citylink Tollway beyond. Continuity of this western barrier, generally an average height of 6 storeys, is a primary element for the Freeway Apartments precinct. The deep setback from the street, averaging 20.0m, allows for expansive and elevated views over the top of the Street-edge Apartments and towards the Heritage precinct and parkland to the north-east. The Spine apartments connect the Tower type 01 buildings. Expressed horizontal ribbons of terraces/spandrels, to the east facade of the spine apartments, act as an abstract backdrop to the form and facade treatment of the adjoining towers.

Section A

Indicative east elevation (Stage 7)

Tower typology entry access/control "Portal"



Section A (Stage 7)

Section A

Ground Floor plan (Stage 7)

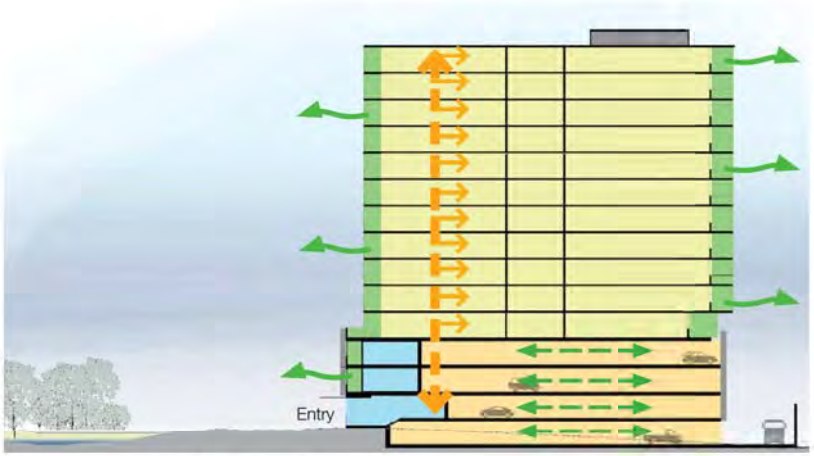
Tower type 01

Public domain

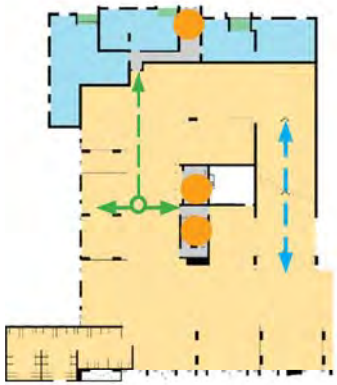
- The main public entry is addressed directly off the public footpath along Galada Avenue.
- Edge treatment should work to emphasize the connection to the primary entry from the public footpath and be more formal in nature.
- The primary entry into the tower buildings will be framed to emphasize its presence in the street and take its place among a "community of entrances"
- The physical and visual presence of the tower buildings on the street create a clear point of reference for pedestrian access.

Legend

- Tower apartments
- Resident Parking
- Private open space
- Public open space
- Street-Edge Apartments
- Pubic circulation/node
- Semi-public circulation/node
- Vertical circulation



Section at tower (Stage 7)



First Floor plan (Stage 7)

Residential amenity

- The main entry lift lobby should be a secure space with resident access control.
- Within the tower apartment typology the primary means of access is through the lift core. The core provides access for residents, services and removals. The lift can use a double door configuration to resolve level differences between natural ground level, car parking and habitable levels.
- Bicycle storage is to be provided in a secure area. Guest bicycle storage racks ~~should~~ will be provided near the main entrance of the tower.
- Car Parking for the tower residents is to be provided within the car parking decks and will typically be located on the lower basement level.
- Visitor car parking is to be provided both in the form of kerb side parking along the street and within the parking decks.
- Tower apartments that interface with the podium deck have private courtyards.

Podium plan (Stage 8)

Ground Floor plan (Stage 7)

Legend

- Tower apartments
- Resident parking
- Private open space
- Public open space
- Street-edge Apartments
- Vehicle related pedestrian traffic
- Level access
- Vertical circulation core
- Natural ventilation

Indicative east elevation (Stages 5-6)

Section A (Stage 6)

Ground Floor plan (Stage 5-6)

Tower type 02

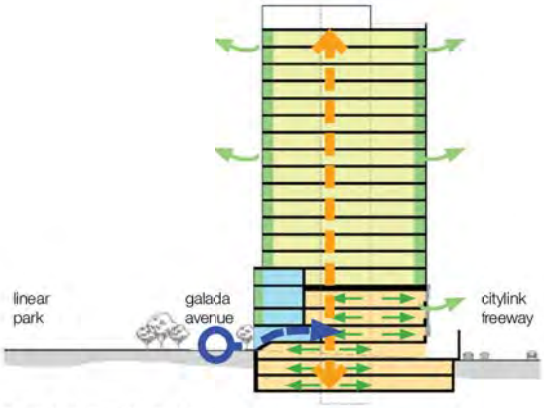
Public domain

- The main public entry is addressed directly off the public footpath along Galada Avenue.
- Edge treatment ~~should work to~~ emphasizes the connection to the primary entry from the public footpath and is formal in nature.
- The primary entry into the tower buildings is framed to emphasize its presence in the street and take its place among a "community of entrances"
- The physical and visual presence of the tower buildings on the street create a clear point of reference for pedestrian access.

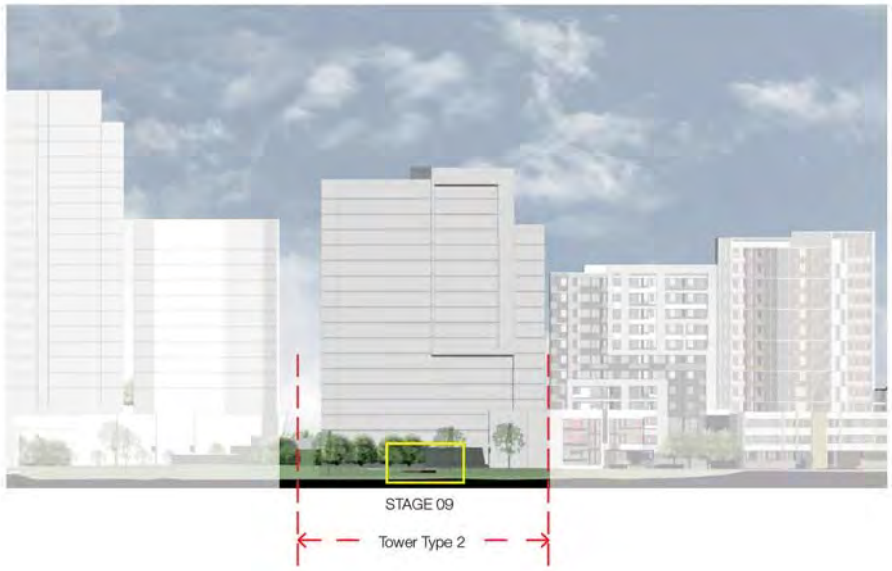
Legend

- Tower apartments
- Resident Parking
- Private open space
- Public open space
- Street-edge Apartments
- Pubic circulation/node
- Semi-public circulation/node
- Vertical circulation

Tower type 02 - STAGE 9



Tower Section A (Stage 9)



Tower Type 2 (Stage 9)

Tower Type 2 (Stage 9)

Legend

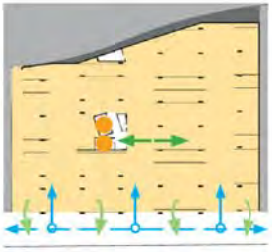
- Tower apartments
- Resident Parking
- Private open space
- Public open space
- Street-edge Apartments
- Pubic circulation/node
- Semi-public circulation/node
- Vertical circulation

Section A

First Floor Plan (Stage 5 - 6)

Ground Floor Plan (Stage 5 - 6)

Section A



Basement plan (Stage 6)

Tower type 02

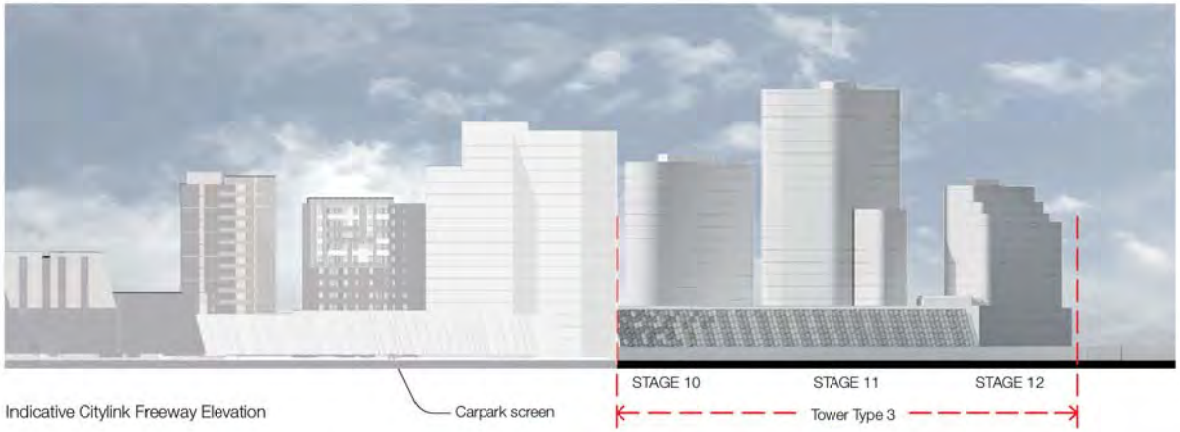
Residential amenity

- The main entry lift lobby ~~should be~~ is a secure space with resident access control.
- Within the tower apartment typology the primary means of access is through the lift core. The core provides access for residents, services and removals. The lift can use a double door configuration to resolve level differences between natural ground level, car parking and habitabl e levels.
- Bicycle storage is ~~to be~~ provided in a secure area. Guest bicycle storage racks ~~should be~~ are provided near the main entrance of the tower.
- Car Parking for the tower residents is ~~to be provided~~ within the car parking decks and ~~will~~ is typically be located on the lower basement level.
- Visitor car parking is ~~to be provided~~ both in the form of kerb side parking along the street and within the parking decks.
- Tower apartments that interface with the podium deck have private courtyards.

Legend

- Resident Amenity
- Resident parking
- Private open space
- Public open space
- Street-edge Apartments
- Vehicle related pedestrian traffic
- Level access
- Vertical circulation core
- Natural ventilation

Indicative Galada Avenue Elevation



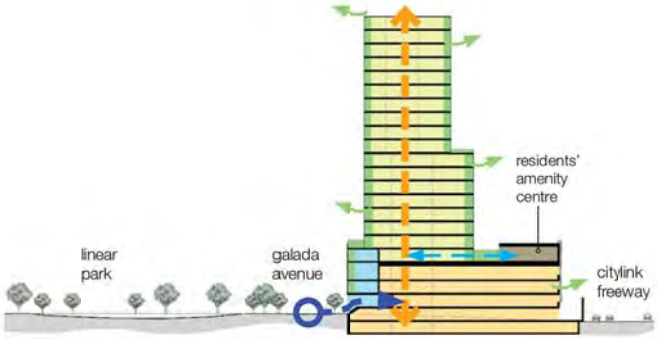
Indicative Citylink Freeway Elevation

Tower type 03

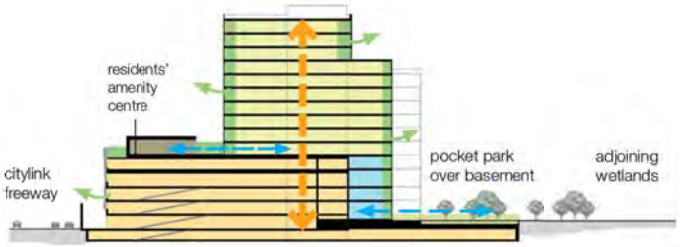
Public domain

- The main public entry is addressed directly off the public footpath along Galada Avenue.
- Edge treatment should work to emphasize the connection to the primary entry from the public footpath and be more formal in nature.
- The primary entry into the tower buildings will be framed to emphasize its presence in the street and take its place among a "community of entrances"
- The physical and visual presence of the tower buildings on the street create a clear point of reference for pedestrian access.

Ground floor plan (Tower Type 3)



Tower Section A (Stage 11)

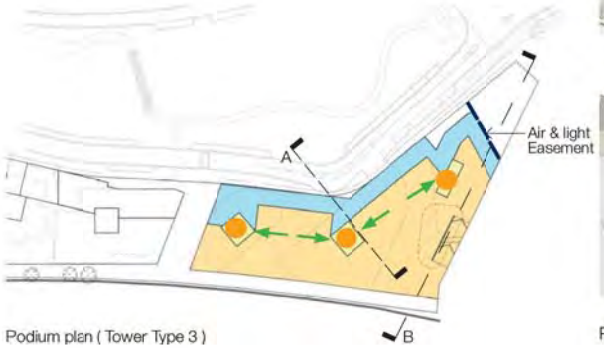


Tower Section B (Stage 12)

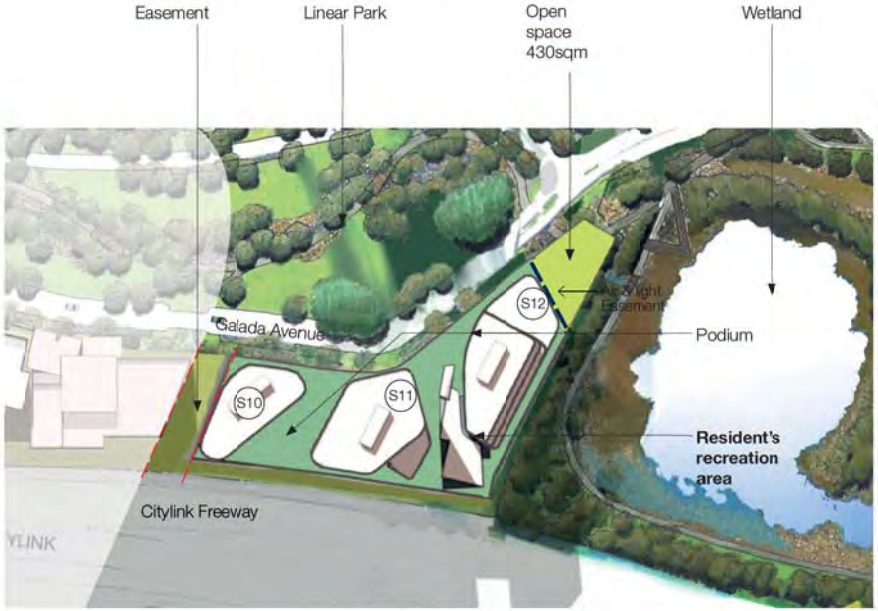
Residential amenity

- The main entry lift lobby should be a secure space with resident access control.
- Within the tower apartment typology the primary means of access is through the lift core. The core provides access for residents, services and removals. The lift can use a double door configuration to resolve level differences between natural ground level, car parking and habitable levels.
- Bicycle storage is to be provided in a secure area. Guest bicycle storage racks should be provided near the main entrance of the tower.
- Carparking for the tower residents is to be provided within the car parking decks and ~~will typically be located on the~~ lower basement levels as required.
- Visitor car parking is to be provided both in the form of kerb side parking along the street and within the parking decks.
- Tower apartments that interface with the podium deck have ~~private courtyards~~ larger terraces.

Undercroft plan (Tower Type 3)



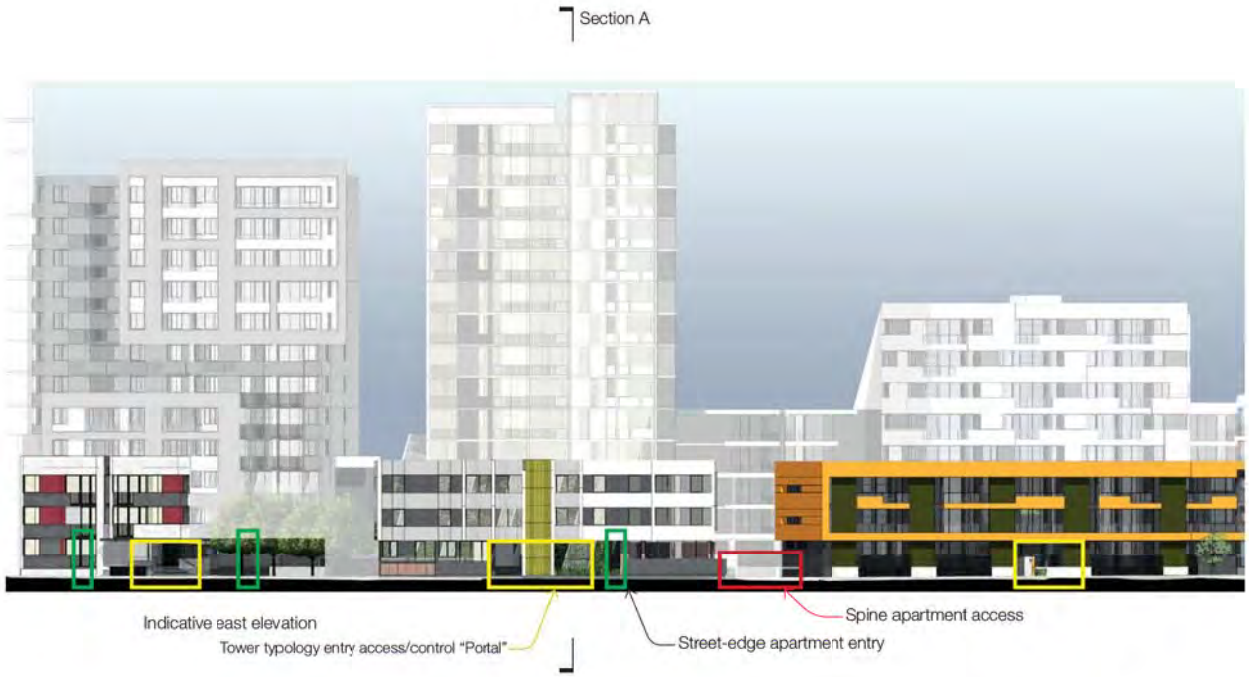
Podium plan (Tower Type 3)

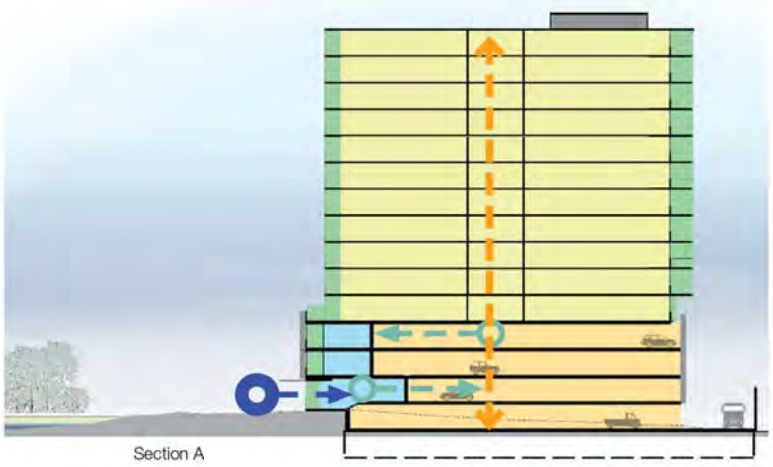


Podium terrace Residents' amenity centre, Pocket park + surrounds

Legend

- Tower apartments
- Resident parking
- Private open space
- Public open space
- Mixed use space
- Residents' amenity centre
- Street-edge apartments
- Vehicle related pedestrian traffic
- Level access
- Vertical circulation core
- Public circulation/node
- Semi-public circulation/node
- Vertical circulation





Street-edge apartments

Public domain

- The entry to the single aspect units that interface with the podium parking structure, ~~the entry~~ is addressed directly off the footpath through a private courtyard where practical. Entry is from the podium to dual aspect units that sit level with the podium deck ~~entry is from the podium~~. The podium ~~is to~~ may be accessible via a ~~external~~ dedicated stair with a controlled entry point from the main public footpath.
- Edge treatment should work to emphasize the connection to the podium and develop visual links to the podium units.
- The primary entry into the street-edge apartments will be identifiable from the street and adjacent roadway.
- Apartments located at the street-edge provide direct access to the footpath with rear access facilitated from the podium deck.

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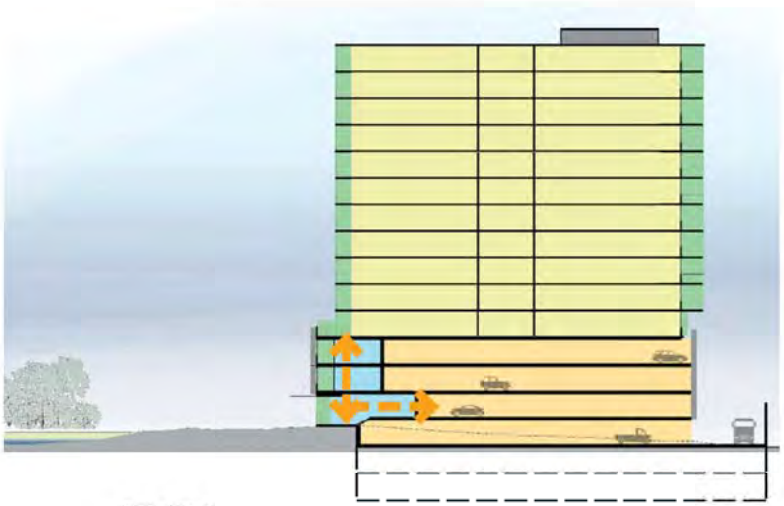
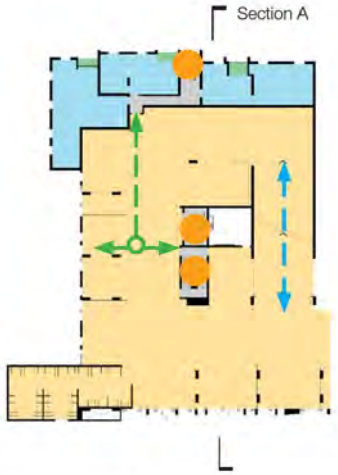
Legend

- Street-edge apartments
- Resident Parking
- Private open space
- Public open space
- Public circulation/node
- Semi-public circulation/node
- Vertical circulation
- Vehicle related pedestrian traffic
- Level access



Typical section



First Floor plan (stage 07)

Ground Floor plan (stage 07)

Residential amenity

- Within the street edge apartment typology the primary means of access is off the public / semi-public space
- Bicycle storage is to be provided in a secure area. Guest bicycle storage racks should be provided near the main entrance of the tower.
- Car Parking for the street edge apartments residents is to be provided within the car parking decks, typically on the ~~upper basement plan~~ podium levels.
- Vehicle related pedestrian access off the parking deck is to be through the spine core or where possible directly into the units.
- Visitor car parking is to be provided both in the form of kerb side parking along the street and within the parking decks.
- Street-edge apartments addressing the roadway should have private courtyard zones that act as a buffer to public/private interface
- Where applicable Street-edge dwellings that interface with the deck are to have a private courtyard with a secondary rear access off the semi-private podium deck.

Legend

- Street-edge apartments
- Resident parking
- Private open space
- Public open space
- Vehicle related pedestrian traffic
- Level access
- Vertical circulation core
- Natural ventilation

Indicative elevation (Stages 5 -6)
from Galada Avenue

Rear Spine elevation (Stages 5 -6)
from Citylink Freeway
(Freeway noise wall removed for clarity)

First Floor plan (Stage 5 - 6)

Section A

Section A (Stage 6)

Podium plan (Stage 5 - 6)

Spine apartments

Public domain

- The main public entry is ~~off the semi private podium deck into~~ the common vertical circulation core. The podium deck serves as the primary public space interface to the spine.
- Edge treatment should work to emphasize the connection to the podium and develop visual links to the spine units.
- The primary entry into the spine typology should be framed to emphasize its presence in the street so it can take its place among a "community of entrances".
- Where separate from the main tower entry. The podium entry portal should act as the primary visual link to the spine apartments entry.

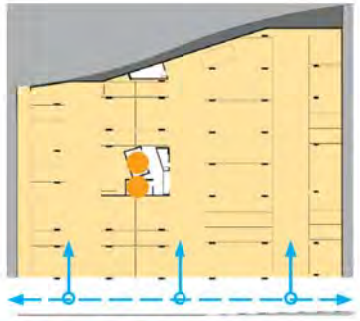
Legend

- Spine apartments
- Resident Parking
- Private open space
- Public open space
- Public circulation/node
- Semi-public circulation/node
- Vertical circulation

First Floor plan (Stages 5 - 6)

Typical Section A (Stage 6)

Podium plan (Stages 5 - 6)



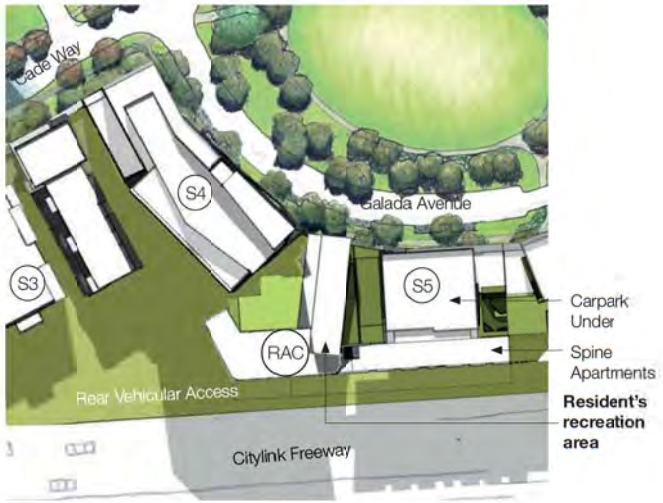
Basement plan (Stage 6)

Residential amenity

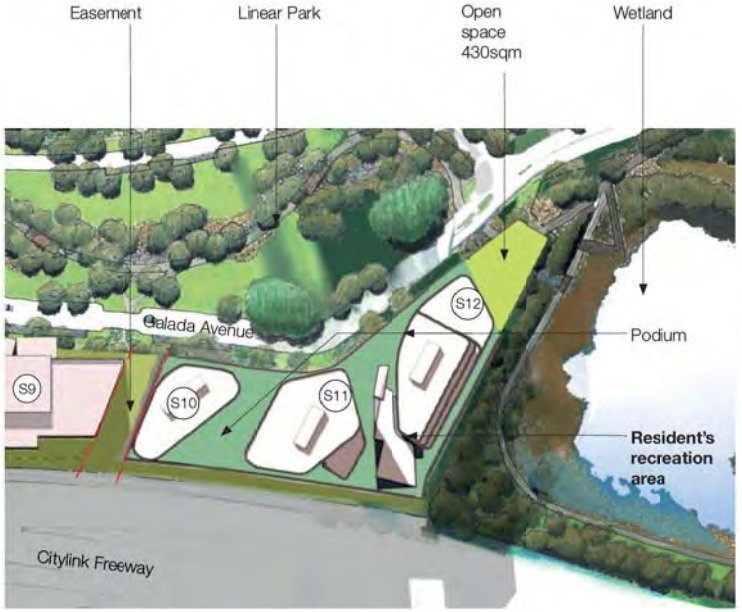
- Within the spine apartment typology the primary means of access is through the vertical circulation core. The core provides access for residents, services and removals.
- Bicycle storage is to be provided in a secure area. Guest bicycle storage racks should be provided near the main entrance of the tower.
- Car Parking for the spine residents is to be provided within the car parking decks, typically on the ~~upper basement~~ undercroft/lower ground plan.
- Visitor car parking is to be provided both in the form of kerb side parking along the street and within the parking decks.
- Spine apartments that interface with the deck are to have private ~~courtyards~~ terraces at the podium level.

Legend

- Spine apartments
- Resident parking
- Private open space
- Public open space
- Vehicle related pedestrian traffic
- Level access
- Vertical circulation core
- Natural ventilation



Northern resident recreation area

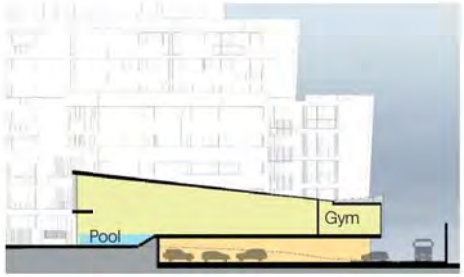


Podium terrace Residents' amenity centre, Pocket park + surrounds

View of Rec Centre (as built)

Northern Rac Centre Ground Floor Plan

Northern Rac Centre Podium Plan



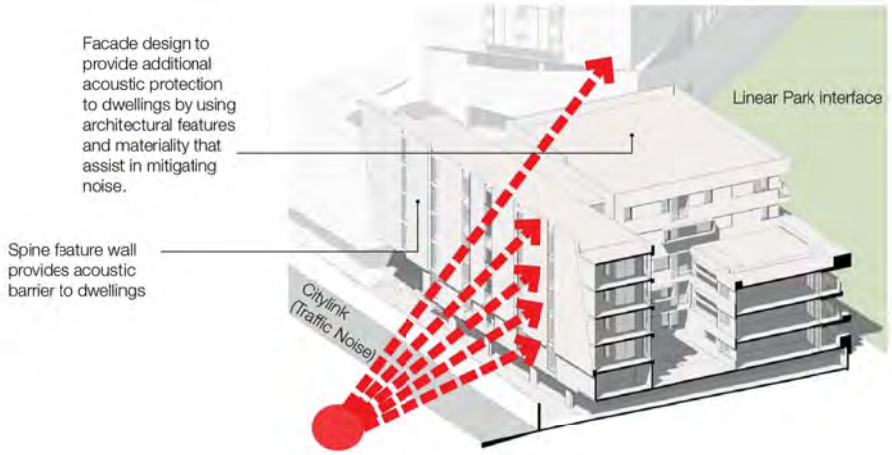
Section A (RAC)

Residential Amenity

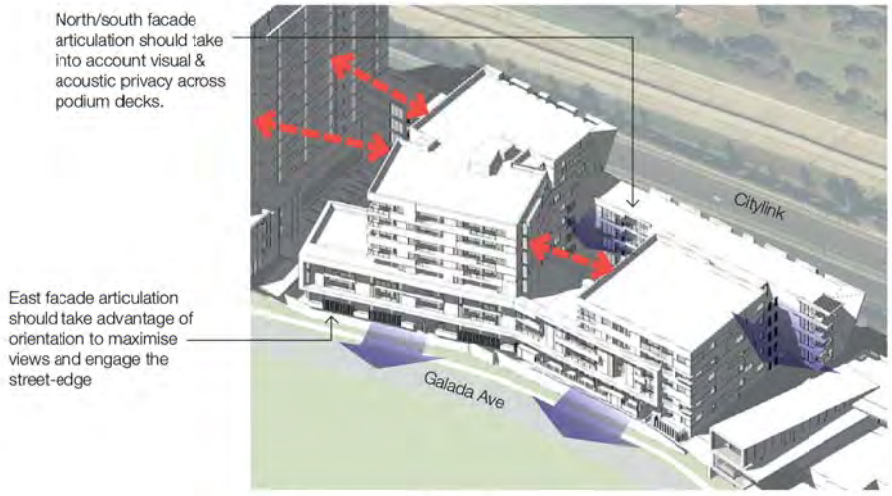
The Freeway Apartments precinct an extension of the Village and Parkland precincts. The proposal contributes to the overall site through the creation of a vibrant, well-designed series of buildings providing a high level of resident amenity.

General amenity

- Adequate passive surveillance of adjacent streets, paths and parklands through appropriate activation of building facades
- A legible hierarchy of public and private spaces
- Clear and well located way finding and signage elements
- High quality landscaped spaces within the apartment building interfaces and overall integration with the ~~Tract Masterplan Guidelines~~ Landscape Plan
- A distinctive design differentiation of stages to foster a sense of ownership and individuality for residents within the Freeway Apartments precinct. This differentiation may extend to the colour palette, materials and finishes as well as the landscape design.
- Access to daylight, sunlight and natural ventilation will need to be considered for each of the proposed apartment buildings.
- All lifted apartments will provide disabled access in conformity with Clause 10.1 of the Incorporated Document
- Secure car parking for all residents with disabled access to all apartments served by lifts
- Secure bicycle storage facilities for residents and visitors in accordance with Clauses 8.8 of the Incorporated Document
- The November 2003 Disability Access Brief controls the requirements for disability access at Parkville Gardens for both Games and post Games development. The Disability Access Brief sets out specific strategies for disabled access and provides detail on how these can be achieved. Section 2.0 of the Brief deals with Housing and hence applies to 'lifted apartments'. Section 2.0 nominates that Lifted Apartments will be Visitable and Adaptable and sets out the specific requirements that will apply to these dwelling types.
- Two resident recreation areas are positioned above the carpark to encourage passive surveillance and positive utilisation of the outdoor open space. The northern resident recreation area is positioned behind ~~Tower~~ Stage 4 & ~~Tower~~ Stage 5, while the southern area will be behind ~~Tower~~ Stage 11 and ~~Tower~~ Stage 12.



Sectional diagram of acoustic strategy



Sectional diagram of acoustic strategy

Acoustic amenity

This precinct site is affected by noise from the Citylink freeway that abuts the western edge of the site. Principles that govern overall building form have been developed to mitigate any acoustic issues:

- The dwellings in the Freeway Apartments precinct should be designed in accordance with the performance requirements of AS3671-1989 "Acoustics - Road Traffic Noise Intrusion - Building siting and construction"
- The principal device used to assure acoustic amenity to residents is the western wall. The height and materiality of the wall will provide an acoustic buffer to the dwellings within this precinct and to the development as a whole.

Stage 8 Entry

Street-edge apartments

Tower apartments

Image & Identity

Architectural expression

A contemporary architectural language will be adopted for the Freeway Apartments precinct. Building forms will be site responsive with overlaid screens, shades, panels and balustrading responsive to the individual orientation.
Expressive elements of texture and colour may be applied over a generally muted, natural colour palette. The facades oriented towards the Heritage precinct and public open space will offer a high quality architectural presentation and a level of passive surveillance, whilst avoiding fully-glazed building envelopes.

Materials and finishes

Materials and finishes within the Freeway Apartments precinct will be selected in consideration of their relation to the preceding project stages and the diverse edge conditions of this site.
A robust selection of materials and finishes with be adopted for the apartments, in contrast to the more traditional palette used in the adjoining residential neighbourhoods.

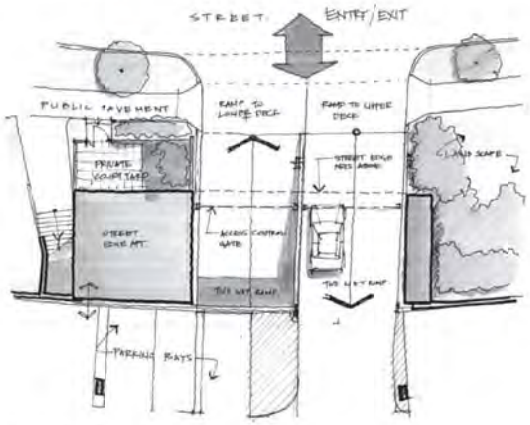
Materials and finishes should include:

- Insitu and precast concrete facade panels with a variety of form liner textures, patterns and colour tints
- Powder-coated aluminium sliding door and window systems with solar and acoustic performance glazing
- Powder-coated and galvanised steel detailing elements for sun-shading, screens and balustrades
- Render and paint finishes applied over concrete and masonry elements
- Selected screens and apartment dividers (in weather-protected zones)
- ~~Pre-~~Painted elements to the street-edge apartment roofs and detailing elements
- Perforated and louvred screens to rooftop plant screens and expressed roof elements

The overall colour palette should complement the base building tones of the established neighbouring precinct. Highlight/accents should be incorporated to emphasise individual buildings/entries/addresses within the precinct.

Building equipment including services, meters, plant and plumbing should typically be screened from view. Rooftop plant and equipment should be integrated as a building design element and consideration given to the visibility of all "roofscapes" from Travancore, the Citylink and particularly the elevated sound-tube.

Pedestrian and vehicular access



Detail of vehicle entry

NOTE:
Vehicle access yields are approximate as final apartments yields are yet to be finalised for the remaining stages. In some instances vehicles will have 2 options for access to the site.

Access / Circulation

Pedestrian access

- Create a safe, pedestrian orientated residential environment, which integrates with the surrounding areas.
- Provide footpaths that are at least 1.5 m wide.
- Provide disabled and safe access to public areas.

Vehicular access and parking

- Provide adequate, safe and efficient car parking for residents and visitors. 80% of visitor parking to be provided at street kerb-side. Car parking for residents should not dominate the streetscape.
- Provide appropriate access to the apartments for service, emergency and delivery vehicles and adequate parking for trades people and removalists
- Reserve 5 meters of land between the apartment buildings and the noise wall along Citylink to provide maintenance access.
- Access points are to be fully integrated into the landscape approach along the street-edge to minimise visual disruption of built form.
- The proposed yield will work with the existing internal and external road network.

The following is advice from Traffic Engineer Chris Maragos and Associates regarding the adequacy of a 7 access point configuration.

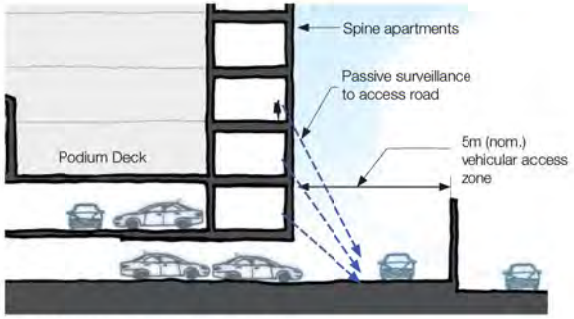
Vehicular access & Parking

A total of 7 access points are proposed for the off street car parking provided under the ultimate development, including 3 provided on Cade Way and the balance provided off Galada Avenue.

Current documentation indicates that the access points provided on Cade Way will provide access to approximately 110 parking spaces. The northern most access on Cade Way will provide access to the rear lane to extend along the western boundary of the site, while the central and southern access points on Cade Way will serve minor/secure resident car parks comprising 19 & ~~35~~ 29 spaces respectively.

Traffic data was collected in 2015 and the results used to update previous work carried out for Parkville Gardens and the Freeway Apartments precinct. The new data indicates commuter peak hour traffic generation rates of around 0.2vph per apartment.

The expected traffic distribution of existing and proposed apartments was reviewed and analysed along the internal roads and at the external intersections. The anticipated change in traffic flows is expected to be adequately accommodated at the precinct access points and at the external intersections.



Detail of section of rear access roadway

Guest parking

Vehicular access and parking

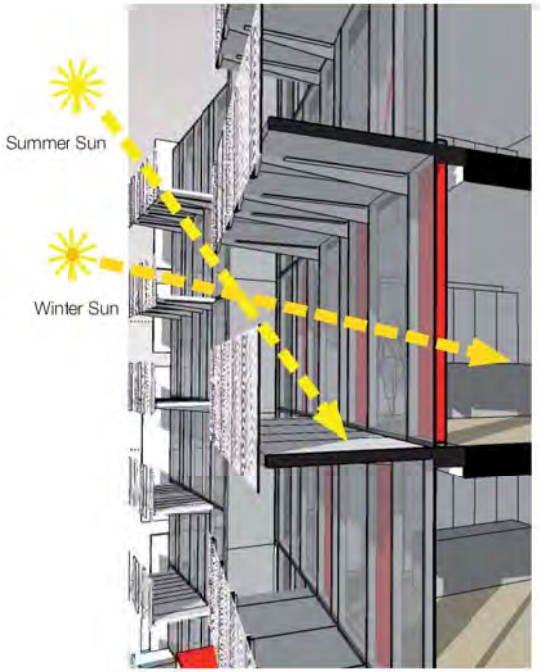
- Passive surveillance over the open space to the rear of the site and adequate lighting and signage create a secure environment for pedestrians and vehicle movements. An example of an existing building which utilises these principles is the Melbourne Museum carpark in Carlton.

Guest parking provision

- Under the provisions of the Parking Precinct Plan (January 2004) visitor parking will be provided at the rate of 1 space per 10 units. 80% of the visitor spaces shall be provided on-street and 20% off-street.
- In ~~Scenario 1~~ the project staging section of the document, the yield analysis requires 20 off street and 84 on street parking spaces. ~~Scenario 2 requires 12 off street and 56 on street parking spaces.~~
- To meet the requirements of the visitor parking provision stipulated in the Precinct Parking Plan, VPC will provide indented on-street parking along both the western side of Cade Way and kerb-lines of Galada Avenue directly in front of the proposed Freeway Apartments precinct. The final number of spaces provided will be adjusted in line with the apartment yields for each stage.
- Off street parking will be accommodated within, clustered around the entry points as to facilitate access and provide adequate separation to the resident parking.

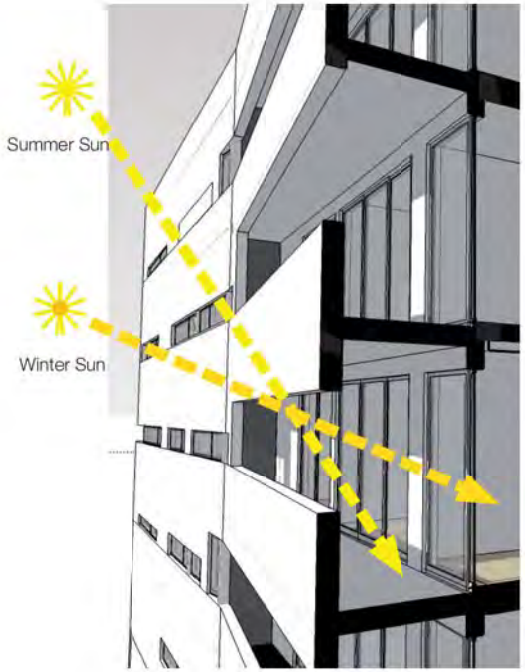
Rear vehicular access

- The rear vehicular access way is conceived as a private resident and service vehicular access roadway only. There will be no through car or pedestrian access in this space.
- Vehicle related pedestrian access will proceed directly into the parking decks and to the corresponding cores. Passive security and oversight will be provided by the spine apartments located above the roadway. (Refer diagram.)
- This access way can be divided into various sections to prevent through traffic. Access through these sections will be possible for Citylink service vehicles.



Sun shading - east elevations

- The objective is to use overhangs and balconies to maximise the amount of shade to dwellings in the summer and sunlight in the winter
- Passive solar design should be utilized to minimise heat gain/ loss by carefully considering the amount and configuration of glazing addressing a certain orientation.
- East Facades should "open up" to promote transparency to the street-edge and reinforce views beyond.



Sun shading - north/south elevations

- The objective is to use overhangs and balconies to maximise the amount of shade to dwellings in the summer and sunlight in the winter
- Passive solar design should be utilized to minimise heat gain/loss by carefully considering the amount and configuration of glazing addressing a certain orientation
- North and south elevations should "close down" in order to moderate heat gain and foster an idea of enclosure/privacy for residents across the podium deck.

Environmentally Sustainable Design

ESD initiatives

The following Environmentally Sustainable Design initiatives for the development of the Freeway Apartments precinct will be considered in order to exceed the prescribed ~~5 Star~~ 6 Star Building average with a minimum of 5 Star as per BCA requirements, energy rating as assessed by the First Rate system:

- Optimization of passive solar design opportunities in relation to:
  - Building siting/orientation
  - Shading to apartment glazing
  - Minimisation of openings to west facades
  - Solar boosted gas hot water
- Specify the use of 'AAA' rated water efficient fittings and appliances on design plans.
- Stormwater catchment and filtration (recycled for landscape irrigation)
- Low maintenance and drought tolerant landscape design
- Thermally efficient building envelope design:
  - High-performance window/door system to façade
  - Thermally insulated wall and roof design
  - Sealed façade to reduce leakage
- Selection of low-energy type light fittings and appliances
- Motion sensors to low-use common areas
- Naturally ventilated resident carpark, with CO^2 sensors fitted to carpark exhaust fan system where possible
- Low embodied energy construction materials selection wherever practicable
- Interior fit-out and finishes to have low volatile organic compound (VOC) rating to improve indoor air quality.

Subject to Government assisted funding, it is also proposed to consider other major additional ESD initiatives including:

- Co-generation electrical power supply
- Photovoltaic solar arrays integrated with the building façade to power common area lighting
- Solar powered street and landscape lighting design
- Geothermal exchange - efficient heating and cooling

Water sensitive urban design

Incorporate Water Sensitive Urban Design (WSUD) techniques into the design of roads, landscaping, public open space and other development having regard to the integrated water management plan and stormwater management system for the site.

- Stormwater runoff from the site will be directed to the existing bio-retention system in the linear park for treatment prior to discharge from Parkville Gardens Village.
- The Bio-retention system has been designed to cater for flows from the Freeway Apartments site which is identified as Catchments C & D in the Integrated Water Management Plan.

Site Plan

Landscape Design

The landscape design for the Freeway Apartments precinct will support the original Development Masterplan objectives, including:

- extending the character of Royal Park into Parkville Gardens
- creating an identifiably Australian aesthetic in the landscape
- fostering the transition to sustainable design.

Located adjacent to Parkville Garden's Heritage precinct and Linear Park, the Freeway Apartments precinct will respect established landscape design parameters.

Soft landscape

- Heritage precinct interface
  The existing streetscape planting will form a structured streetscape edge between the Freeway Apartments and the Heritage precinct. Beyond this formal edge, the landscape within the Heritage precinct interface will adopt the informal, meandering native landscape character of the adjoining Linear Park Interface. Large areas of open lawn are retained along the Heritage precinct interface frontage, strengthening the "park-like" atmosphere established within the Heritage precinct.

- Linear Park Interface
  In keeping with the objective to extend the character of Royal Park into the Village, the Freeway Apartments precinct landscape, along its Galada Street frontage, will seek to reinforce and build on the established planting themes to create the perception of "Apartments within a Park."

  As outlined in the Development Masterplan, trees within this interface will be planted informally, "generally in groups with no distinction between street and parkland planting, visually expanding the park to include the streets, dwelling, and apartment frontages."

  Tree planting will predominantly consist of indigenous species creating a seamless transition between the Freeway Apartments precincts planting within the Linear Park and the MCC Storage Wetland planting at the site's southern boundary. Similarly, the parkland's planting palette of indigenous native grasses, groundcovers, sedges and herbs will also "jump the street" and extend into streetscape and the Freeway Apartments precinct gardens where they will be joined with complementary non-indigenous natives. In this way, the perceived breadth of the park will be expanded and an integrated design aesthetic will be created. Planting will consist of low water demand species suited to their position.

Site Plan

Hard landscape

Hard landscape elements throughout the Freeway Apartments precinct will be consistent with themes established within the Development Masterplan. Well designed outdoor space will be an integral component of the Freeway Apartments open space system, creating a hierarchy of private to public outdoor areas.

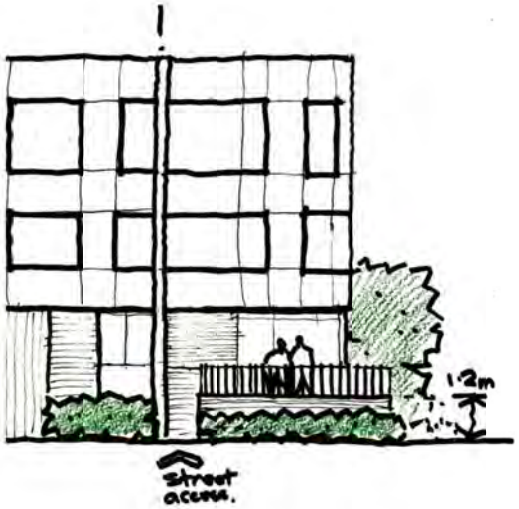
- Private courtyards will provide outdoor spaces directly associated with individual residences.
- Unique semi-private elevated garden zones will provide the opportunity for residents and visitors to enjoy these spaces either individually or as part of a larger group.
- Broad steps will form the transition from semi-private courtyard spaces to the streetscape and the public open space beyond.
- The on-structure planting palette will draw on native plants known to perform in a podium-landscape setting, appropriate to soil depths, wind conditions, and solar access.

Sustainable landscape design

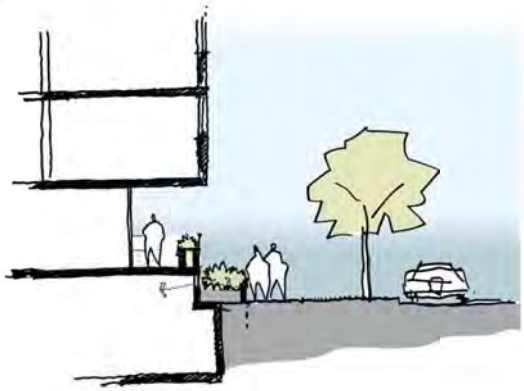
As per the precedence established in previous development stages, in adopting the vocabulary of the Australian environment within the Freeway Apartments precinct landscape design, the site will also enhance flora and fauna biodiversity.

Similarly, the Freeway Apartments precinct will use less water and chemical control than conventional landscapes and the precinct's stormwater will be cleaned using the existing "ephemeral creek" or infiltration zone established within the adjacent linear park before allowing its transfer to receiving waters.

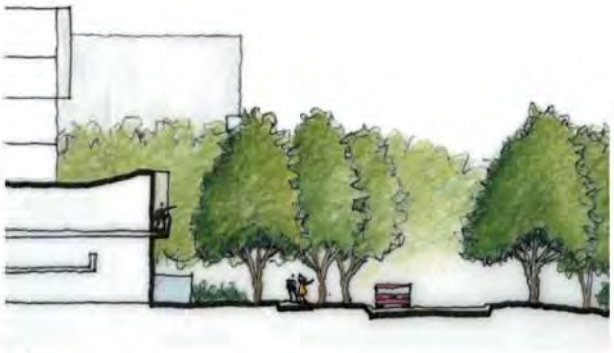
The Freeway Apartments precinct stormwater catchment and filtration (recycled for landscape irrigation) will also be incorporated into the site's architectural and landscape design.



Street edge condition Elevation



Minimum edge setback section



Extended edge setback section

Landscape edge treatments

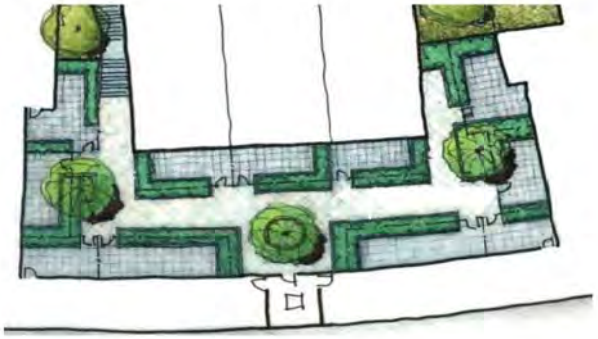
~~The freeway apartments generally sit within a generous landscape of undulating lawn and garden beds (refer Section – Extended setback) reinforcing the development's established "park-like" setting.~~
~~Where there is a need to provide privacy to residents' courtyard gardens (along the street frontage of proposed Street-edge apartments and on podiums), 1.8m high fences are included within a planted garden setting.~~

- ~~Where fences are included within the streetscape frontage, a minimum 1.2m width planted setback softens their streetscape presentation.~~
- ~~On podiums, fences and raised planters are combined to provide privacy to residents' courtyards, while also offering shade, shelter, and desireble green-space to both semi-private and private garden spaces.~~

Integrated landscaping opportunities are provided at ground level within the breaks between each stage and at podium interface with the streetscape, responding to the development's established "park-like" setting.

Where there is a need to provide privacy to residents' courtyard gardens (along the street frontage of proposed Street-edge apartments and Tower Type 3 Apartments) fencing with 50% opacity is provided above the finished floor level of the ground level apartments facing Galada Avenue.

On podiums, fences and raised planters are combined to provide privacy to residents' courtyards, while also offering shade, shelter, and desirable green-space to both semi-private and private garden spaces.



Podium landscape plan

Podium landscape section

Landscaped decks

Apartment decks will incorporate a combination of hard and soft elements to delineate private and semi-private spaces and to create amenable outdoor rooms and/or entry corridors to podium apartments.
The landscape layout and design will respond to each podium's unique spatial configuration and character, generally adhering to the following principles:

- Wherever possible, semi-private spaces will include raised planters at edges and/or centrally to create spatial separation, shade/shelter, screening, and visual/sensory interest (texture, colour, scent).
- Paving will typically consist of concrete unit pavers in keeping with internal architectural finishes. Generally larger format pavers to semi-private spaces with slightly smaller pavers in private gardens. Contrasting colours, bands of colours, and/or surface finishes may be used to assist in providing spatial delineation and orientation cues.
- As noted in "Landscape Edge Treatments", fences and raised planters will be combined to provide privacy to private courtyards. Raised planters of rendered concrete/masonry will be included within private gardens wherever possible to soften and provide visual relief and variety. Fence and raised planter design and finish will be in keeping with architectural design/detailing.
- Plants included within raised podium garden beds will typically be native species suited to their position/micro-climate within the podium setting. Plants will generally be hardy, drought tolerant, low maintenance trees, hedging shrubs, strappy plants, climbers and groundcovers. (Refer landscape masterplan for indicative species lists). Care will be taken to ensure visual permeability within semi-private spaces for maximum safety.
- Podium deck landscaping design needs to consider the ESD implications/benefits of any landscape approach. Consideration of micro climate, evaporative cooling and thermal insulation should form some of the criteria in evaluating any landscape design.

Indicative project staging

		Apartment Yield and Mix Analysis									
Stage	Building	Habitable Storey	1 Bedroom (50m2)		2 Bedroom 1 Bathroom (65m2)		2 Bedroom 2 Bathroom (75m2)		3 Bedroom		Total Yield
			No.	%	No.	%	No.	%	No.	%	
1	1	3	5	21%	8	33%	11	46%	0	0%	24
	2	2	0	0%	2	25%	6	75%	0	0%	8
	3	3	6	46%	5	38%	2	15%	0	0%	13
	4	2	0	0%	2	50%	2	50%	0	0%	4
2	5	3	5	21%	8	33%	11	46%	0	0%	24
	6	2	0	0%	2	25%	6	75%	0	0%	8
	7	3	9	47%	6	32%	4	21%	0	0%	19
3	8	3	7	19%	12	33%	14	39%	3	8%	36
	9	3	8	36%	0	0%	13	59%	1	5%	22
4	10	11	67	39%	51	30%	54	31%	0	0%	172
5	11	5	20	35%	25	44%	9	16%	3	5%	57
6	12	7	20	22%	49	54%	19	21%	3	3%	91
7	13	11	41	31%	68	51%	25	19%	0	0%	134
8	14	9	38	47%	18	22%	25	31%	0	0%	81
9	15	15	85	45%	57	30%	47	25%	0	0%	189
10	16	13	74	73%	27	27%	0	0%	0	0%	101
11	17	19	74	42%	37	21%	67	38%	0	0%	178
12	18	10	50	39%	24	19%	53	42%	0	0%	127
Total Yield			509	39.5%	401	31.1%	368	28.6%	10	0.8%	1288
Resident Cars*											1298
Visitor Cars											129

Notes

* 1 car per 1 and 2 Bedroom apartment, 2 cars per 3 Becroom apartment

Approximate yields for Stages 9-12 and may vary dependent on market demand.

5

Project Staging

A staging strategy is to be implemented for the Freeway Apartments precinct that:

- commences with the first stage at the northern end beside the existing social housing building and generally delivers subsequent stages in a southerly direction
- minimises negative impact on existing residents during the construction of later stages
- ensures good buildability of each stage, particularly with regard to access
- creates an attractive freeway 'wall' as noted in the Incorporated Document incorporating flexibility to vary subsequent stages.

- Indicative yields

The Proposed Project Staging diagram indicates that the Freeway Apartments will be delivered in 12 stages. ~~Depending on market demand these stages maybe further broken down into sub-stages or stages may be combined. Analysis has been undertaken on the likely yield of the Proposed Project Staging diagram and depending on the mix of apartments the resulting yield up to 1300 apartments for Scenario 2.~~ The yield analysis has been uncertaken based on the approved planning consents for stages 1 - 8 and the likely proposed yield for stages 9-12, resulting in a total yield of 1288 apartments. The proposed mix and yield for stages 9-12 will be dependent on market demand and may vary.

- ~~Scenario 1~~ Proposed Yield Mix Summary

- This option is made up of predominately smaller 1 Bedroom ~~(51%)~~ (39.5%), ~~and~~ 2 Bedroom ~~(47%)~~ (59.7%) and 3 Bedroom (0.8%) apartments. An indicative mix and sizes of the ~~1041~~ 1288 apartments is indicated in Figure 1.

- Social housing

~~The Village Park Consortium has an obligation to provide a further 18 Social Housing dwellings at Parkville Gardens.~~ Subject to funding, Parkville Gardens may include up to 20% of the final Total Yield as Social Housing. Social Housing has been built in earlier phases of Parkville Gardens and, subject to funding, the balance of the Social Housing units are ~~It is~~ proposed ~~that these will~~ to be included in Stages 9 & 10. The final configuration of these dwellings will be agreed with the Office of Housing prior to the commencement of Stages 9 & 10.

Traffic assessment

Executive Summary

To analyze the proposed traffic conditions at Parkville Garders ~~Cardno Grogan Richards~~ Chris Maragos has undertaken surveys of the existing traffic conditions and adopted these for the ultimate residential component of the village. The result of this aaSIDRA computer analysis indicates that all intersections ~~are anticipated~~ continue to operate at ~~Excellent to Very Good~~ acceptable operating conditions. ~~these are the two highest aaSIDRA operating conditions.~~

Traffic data was collected at Parkville Gardens in October 2014 and May 2015. The data was used to update previous work undertaken for the ultimate development of the precinct, including an analysis of intersection operating conditions and expected traffic flows.

The analysis indicates that traffic flows associated with the ultimate build out of the precinct can be accommodated at the external accesses without adversely affecting passing traffic flows or intersection operating conditions.

The anticipated traffic flows are also expected to be readily absorbed along the internal road network and at the precinct access points.

The resultant traffic along the internal roads is within the desirable limits according to the function of each road as envisaged in the Masterplan.

~~These results also reveal that during peak periods Oak Street would face delays by vehicles turning into the site at both Strickland Avenue and Galada Ave. Cardno's suggest consideration is given to the introduction of a short right turn lane (north to west) on Oak Street at both the Strickland and Galada intersections, these can easily be accommodated within the existing pavement widths and could be readily delineated with line marking.~~

Infrastructure assessment

The site is generally well serviced from an infrastructure viewpoint. The Games Village site as a whole was designed to operate during the period of the games for a population of 6000 athletes and officials with very high peak demand loads on water supply and sewerage due to the high accommodation rates in each of the houses during the games period.

The current load from the residential precinct is significantly reduced thus the main supply infrastructure has significant additional capacity to cater for the construction of the proposed Freeway Apartments.

The following is a brief description of each of the essential services and the ability to accommodate the proposed development. It should be noted that this commentary is general and further detailed design and investigations will be required once the exact apartment numbers are finalised.

- Sewerage
  The site is well served with regard to sewerage assets. Melbourne Waters Moonee Ponds Creek Main Sewer runs through the site and is a 900mm diameter asset. During the development of the village masterplan a connection was ~~has been~~ made to this sewer and allowance has been made for a dedicated sewer branch to the apartment buildings. This is currently located in the linear park area and will require extension to the complex once the layout is finalised. This sewer has capacity to sewer the entire complex however the Northern end of the freeway apartments can also be served by existing reticulated sewers installed to serve the Heritage precinct.

- Drainage
  The drainage masterplan for the site also catered for connection of piped underground drainage to discharge to the bioswale in the linear park. The drainage system will need to be constructed in conjunction with the road that runs adjacent to the Freeway Apartments precinct. 100Yr Flows will also be directed to the linear park and are then conveyed to Melbourne Waters pipeline which discharges directly to the Moonee Ponds Creek on the West side of the freeway.

- Water reticulation
  Water supply was a major consideration during the period of the Commonwealth Games and has resulted in the Village having watermains in excess of that required for "standard" residential development. The freeway apartments will be served by a 225mm diameter reticulated water supply. During the period of the games an upgraded supply pipeline was constructed along Poplar Avenue to the East to ensure continuity of supply for the games mode operations. This infrastructure has remained in place and will ensure that adequate water supply is available.

- Electricity
  Electrical infrastructure exists within the Village to supply the proposed apartment complex. Substations exist at the southern end of the site and also at the Northern end of the Heritage complex. Citipower has been aware that the freeway apartments were always proposed however the exact numbers and electrical loading has never been determined. It has always been proposed to incorporate a further substation into one of the apartment buildings to provide sufficient electrical supply, the sizing and location of this asset will need to be determined by consultation with Citipower. The Parkville site falls midway between two supply zones. High Voltage supply infrastructure exists that was previously used to supply the dining facility that was located on the proposed freeway apartment site, this infrastructure can be used to feed a new substation.

- Communications
  Given the additional media and security requirements in games mode a significant amount of communications infrastructure has been constructed that is now mostly redundant. The conduits for these assets could be utilised to supply the new apartment complex. Conduits will need to be extended along the new road abutting the freeway apartments.

- Gas
  A dedicated gas supply pipeline was provided during the period of the games to supply the dining facility. This terminates approximately midway along the proposed freeway apartment complex. Whilst gas loads would need to be determined this is the logical point to supply gas to the new apartments.

- Summary
  The site is well served to cater for the new complex. Loads for gas and electricity would need to be determined and assessed by the relevant authorities however these services aside all other base infrastructure has been installed and is located adjacent to the proposed development.

Bayslore & Citta Village Park Pty Ltd

Parkville Gardens

Traffic Report

Traffic Engineering Review of the Masterplan

Ref:CM14019
Chris Maragos & Associates
18 April 2016

CMA

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Authorised	

Table of Contents

Figures

1 Background & Introduction

Chris Maragos & Associates Pty. Ltd. was retained by Bayslore & Citta Village Park Pty Ltd to assess the traffic implications of the proposed increase of dwellings within the Freeway Apartment Precinct at Parkville Gardens.

Amendment C115 introduced the Incorporated Document to the schedule to clauses 52.03 and 81 of the Melbourne Planning Scheme called" The Games Village Precinct, Parkville, September 2006. The document specifies the location of Freeway Apartment precinct as well as, the parking requirements, including bike parking for its development.

The original Masterplanning work completed by Grogan Richards in 2006 envisaged about 680 apartments within the freeway precinct, and a total traffic generation of 6,085 vehicles per day for the entire Village.

As of April 2016, Parkville Gardens has seen a significant level of development completed, including 226 medium density dwellings, 82 apartments outside of the freeway precinct, 306 apartments within the freeway precinct, an aged care facility and associated independent living units, and a 90 place childcare centre. Construction is underway, or approvals have been obtained, for the construction of a further 387 apartments within the freeway precinct.

The current planning envisages about 693 apartments within the freeway precinct, with a further 630-680 apartments the subject of future town planning applications.

This report considers the impact of the additional 630-680 apartments in the context of the original Masterplan, and current conditions on the adjacent and nearby road network.

In the course of undertaking this assessment the subject site and its environs have been inspected, plans of the proposed development examined and relevant data collected and analysed. This report builds on the work originally done by Cardno Grogan Richards in October 2003, and which is referred to within the Siting and Development Guidelines for Parkville Gardens.

2 Existing Conditions

2.1 Site, Location & Land Use

The Freeway Apartments precinct, occupies an area of approximately 2.2ha and is located within Parkville Gardens. Parkville Gardens is generally bounded by Park Street to the north, Oak Street to the east and Citylink to the western boundary.

Land use to the north and west of the Parkville Gardens site is predominantly residential. Other development includes the Reggio Calabria Club to the northwest, the CSL, Parkville Youth and Juvenile Justice Centre to the east, the Mental Health Institute to the south and Royal Park sporting fields, golf course and the Melbourne Zoo to the east and southeast of the site.

Parkville Gardens is also predominantly a residential development, but as previously mentioned includes the Mercy Health Aged Care facility, a convenience store and a childcare centre.

Figure 1 shows the location of the site and surrounding road network.

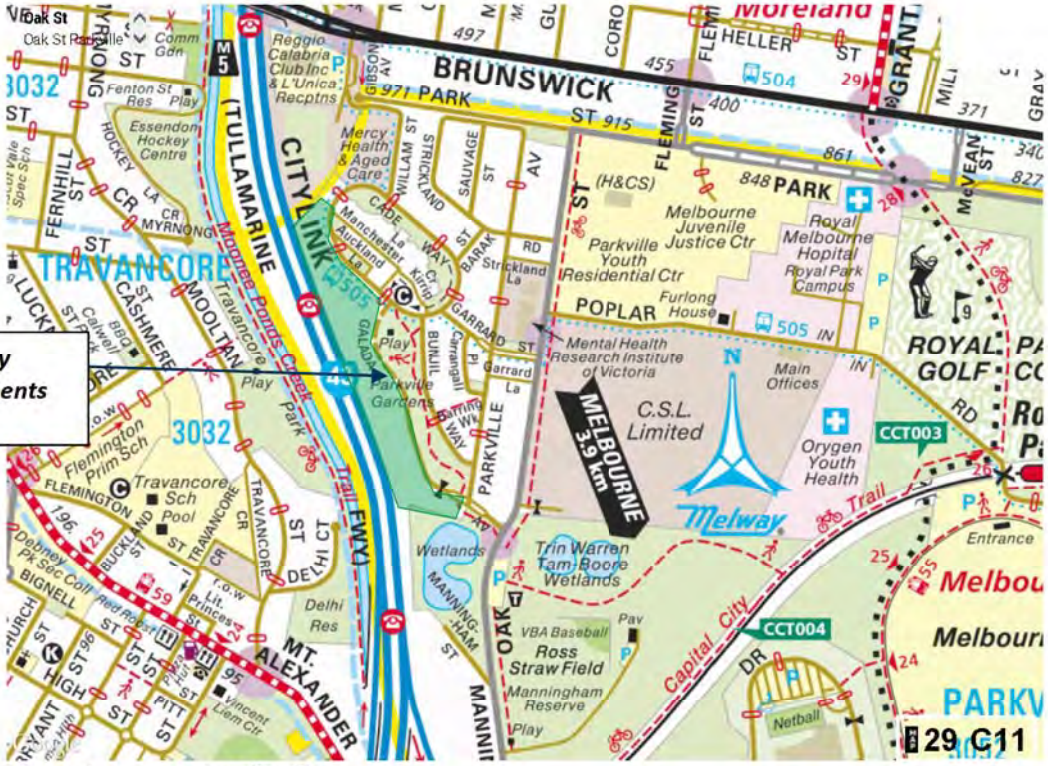
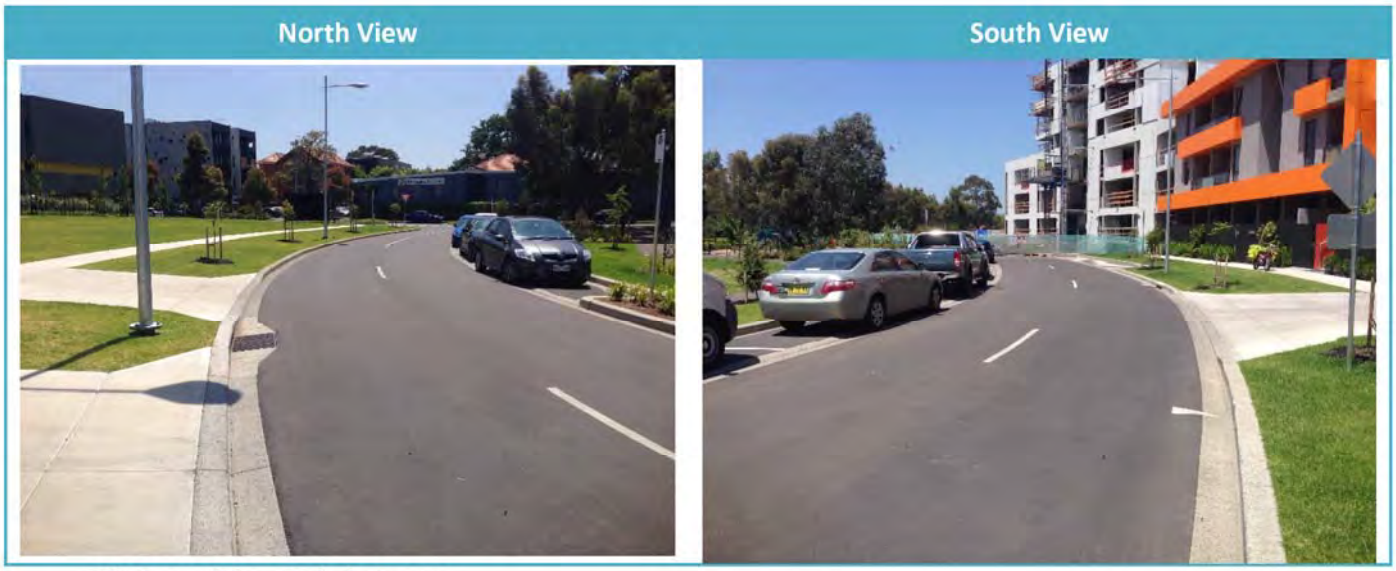


Figure 1 Locality Plan

2.2 Road Network

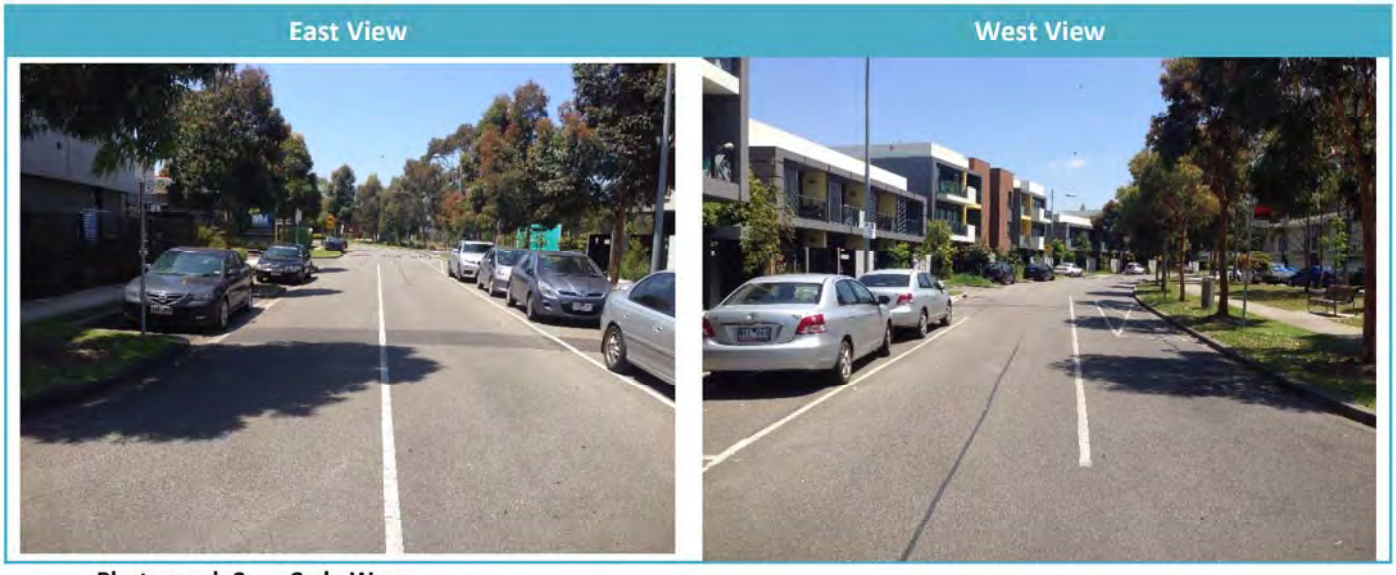
Galada Avenue is being completed and will link Cade Way to Oak Street via the frontage of the Freeway Apartments site. The road caters for a single lane of traffic in each direction plus parking adjacent to each kerb. Photograph 1 illustrates its typical cross section.



Photograph 1 Galada Avenue

Cade Way - Garrard Street, Cade Way circulates the heritage precinct and provides the public transport link from Oak Street via Garrard Street to Willam Street and onto Brunswick Road via Gibson Avenue. Cade Way provides for a single lane of traffic in each direction plus indented parking along its length. Garrard Street flares out to provide separate left and right turn lanes at Oak Street intersection.

The typical section of Cade Way and Garrard Street is illustrated in Photographs 2 and 3.



Photograph 2 Cade Way

East View

West View

Photograph 3 Garrard Street

Oak Street serves a collector/distributor road function and provides a through traffic route from Park Street in the north to Flemington Road in the south. It caters for a single lane of traffic in each direction with painted right turn lanes at intersections adjacent to Parkville Gardens.

Photograph 4 illustrates the typical section of Oak Street.

North View

South View

Photograph 4 Oak Street

2.3 Public Transport

Bus route 505 travels along Cade Way through Parkville Gardens and runs between Melbourne University and Moonee Ponds. Bus route 504 travels along Brunswick Road about 300m north of the site and runs between Clifton Hill and Moonee Ponds.

Tram route 55 travels through Royal Park running from Bell Street, West Coburg, to the Domain Road interchange. The nearest tram stop is located adjacent to Royal Park Railway Station about 1km to the east of the site. In addition the Flemington Bridge Railway Station is located about 900m to the south of the site.

Footpaths and signalised crossings are available to assist pedestrians across Oak Street to and from the nearest tram, bus or railway station.

2.4 Bicycles

The Capital City trail and Moonee Ponds Creek trails are major off-road facilities in the area of Parkville Gardens that provide good cycle connections to the north as well as to the south of the site. An additional off-road path is located along the east side of Oak Street. On-road lanes are provided on Park Street and Royal Parade. On-road lanes are also available in Flemington Road.

3 Surveys

3.1 Traffic Data

Traffic data was collected on various roads within Parkville Gardens from Friday 17 October 2014 for one week. In addition turning movement counts were undertaken at the intersection of Garrard Street and Oak Street on Thursday 23 October 2014 between 7-9am and 4-6pm. The results of the data collected is summarised in Table 1 and Figure 2.

Table 1 Summary of Observed Flows- March 2010 vs October 2014

Road	Daily Average (vpd)		AM Peak Hour (vph)		PM Peak Hour (vph)	
	2010	2014	2010	2014	2010	2014
Oak Street	9,613	8,700*	1,033	768	1,037	806
Willam Street	678	1,457	74	107	52	168
Strickland Street	843	843	68	68	65	77
Garrard Street	999	1,884	112	159	74	209
Galada Avenue	308	561	24	49	27	47

**- estimated*

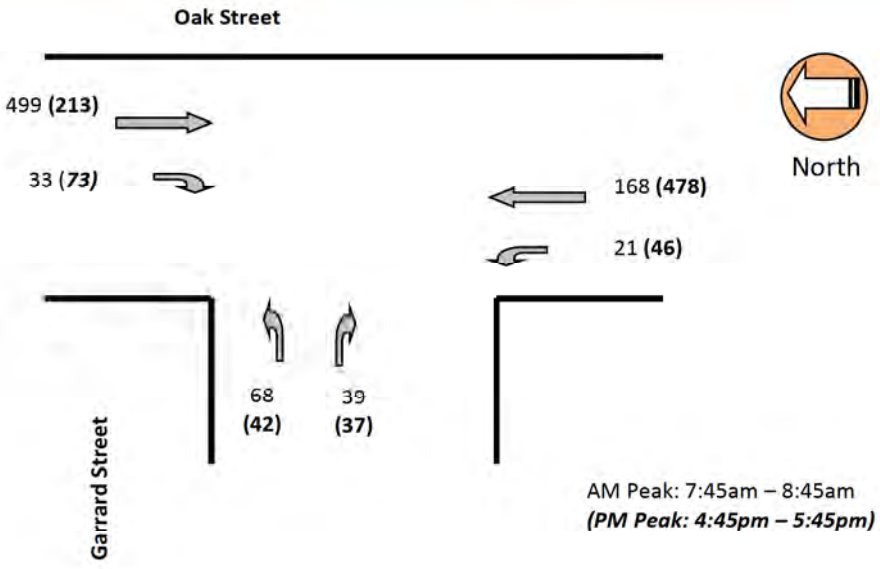


Figure 2 Oak Street/ Garrard Street – Commuter Peak Hour Flows

Table 1 also summarises the traffic data collected by Cardno in March 2010 and shows that there has been a significant increase in traffic on Garrard Street, Willam Street and Galada Avenue, but a decrease in traffic along Oak Street. Traffic along Strickland has remained almost identical to 2010 levels.

The increase in traffic on the internal roads can be attributed to the increase in development that has occurred on the site. The total daily traffic generation on/off the Parkville Gardens site was measured at 4,745vpd in October 2014.

The observed traffic includes construction and contractor vehicles associated with Stage 6, and therefore represents a higher volume than actually generated by the Parkville Gardens Village. A more realistic estimate of the traffic entering and exiting the site is likely to be 150-200vpd less than recorded at 4,550-4.600vpd. The traffic flows are significantly less than the ultimate envisaged by the Masterplan of 6,085vpd.

In addition a 12 hour count of traffic entering and exiting the car park access points to the freeway apartments precinct was undertaken on Thursday 14 May and Saturday 16 May 2015.

The survey indicated a peak traffic generation rate of between 0.17 and 0.19 vph per apartment during the AM and PM commuter peak hours, with a daily rate of about 1.9 vehicles per apartment. The Saturday survey indicated a peak rate of 0.20 vph per apartment and 2.1vpd per apartment. The observed rate is similar to that observed at other high density residential developments that have good public transport connections and are located relatively close to the Melbourne CBD.

The results of the survey indicate that the rate of 0.5 vph per apartment that was adopted in the original Masterplan report was conservative high, and, if applied to future development would result in a high estimate of future flows.

A copy of the traffic survey summary sheets is included in the appendix.

4 The Proposed Changes to the Masterplan

4.1 Description

As mentioned in the introduction it is planned to increase the number of apartments within the freeway precinct from that currently approved to between 1,310 and 1,360 apartments.

As with previous development within the precinct the future apartments will be a combination of 1 and 2 bedroom dwellings. Future development is to be constructed over several stages. For clarity previous and future stages are summarised in Table 2.

Table 2 Summary of Staged Development

Stage	No of Apartments	Status
1	49	completed
2	51	completed
3	58	completed
4	172	Approved
5	57	completed
6	91	Completed
7	134	Approved
8	81	Approved
9-12	630-680	Subject to future applications

4.2 Access

The Siting and Design Guidelines envisaged a total of 7 access points to parking associated with the freeway apartments.

At present 3 access points have been constructed onto Cade Way, and 1 onto Galada Avenue in accordance with the siting and Development Guidelines. Future stages will add a further 3 access points onto Galada Avenue and bring the total to 7 access points. The existing rear laneway will be progressively extended as future stages are completed, ultimately connecting to Galada Avenue at its southern end.

5 Traffic Considerations

5.1 General

As mentioned previously the original Masterplan work by Grogan Richards estimated an ultimate traffic generation of 6,085vpd for Parkville Gardens. The traffic generation of development, as of the last survey date, is estimated at about 4,600vpd.

The construction of previously approved, under construction, or recently lodged stages totals 478 apartments. Future stages are expected to add another 630-680 apartments (subject to planning approval). The following sections assess the traffic implications of all expected development.

5.2 Generation

Traffic generation rates observed, and documented by others, at other high density residential developments such as NewQuay, Docklands, have shown a range of rates between 0.1 and 0.3 cars per apartment. Recently collected data within the freeway apartment precinct indicates traffic generation rates during the commuter peak hours of 0.17 to 0.19vph per apartment.

Consequently a rate of 0.20vph and 2.0vpd per apartment is considered a realistic estimate of the traffic generation for the future dwellings.

Application of the rate to the previously mentioned 478 apartments associated with stages 4, 6, 7 and 8 equates to 96vph (2-way) during the peak periods, or 956vpd. The addition of the anticipated flows to the 4,600vpd will result in a total of 5,556vpd within Parkville Gardens. The completion of Stages 4, 6, 7 and 8 is expected to bring the total traffic generation from Parkville Gardens to about 90% of the original Masterplan estimate.

5.3 Future Traffic

Future development of Stages 9-12 could add a further 136vph during peak periods and 1,360 vehicles over the entire day. The resulting total flows of 6,916vpd are 831vpd above the 6,085vpd envisaged in the Masterplan.

5.4 Traffic Distribution

The expected increase in traffic flows was assigned to the Garrard Street, Galada Avenue and Willam Street routes according to the relative proportions currently using each road (*conservatively assuming that no additional traffic would use Strickland Road*). The ultimate flows are summarised in Table 3 and include a discount for construction traffic that was present at the time of the surveys.

Table 3 Comparison of Current & Future Daily Traffic

Road	Daily Traffic 2014[1]		Development	Post Development
Strickland Road	843	*18%*	0	843
Garrard Street	1,824	*40%*	1,129	2,953
Galada Avenue	500	*11%*	311	811
Willam Street	1,433	*31%*	876	2,309
Total	4,600		2,316	6,916

The increase in flows is expected to equate to 112vph at Garrard Street, 87vph at Willam Street and 31vph at Galada Avenue near Oak Street.

The impact of the additional flows is expected to be greatest at the intersection of Oak Street and Garrard Street. Figures 3 and 4 summarise the anticipated development flows, including future development, and the post development flows, respectively, at that location.

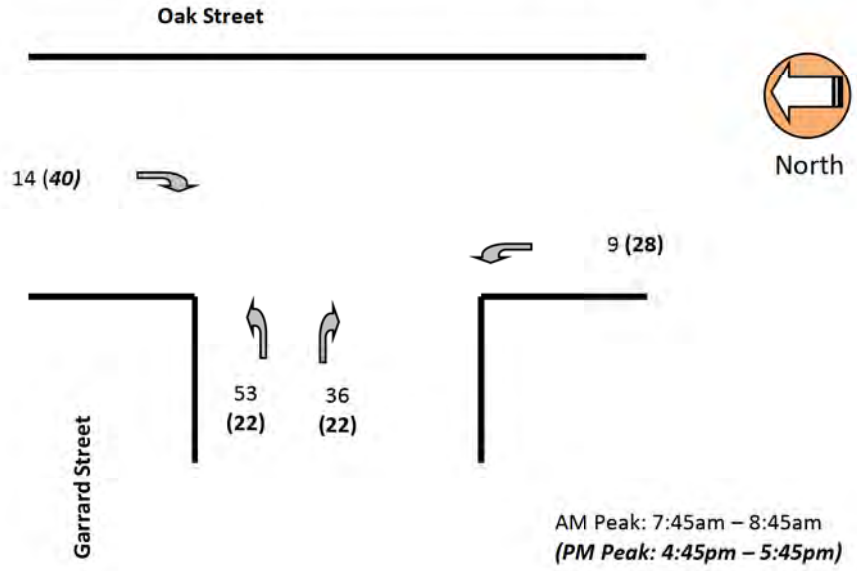


Figure 3 Anticipated Future Development Flows

[1] Adjusted for construction traffic

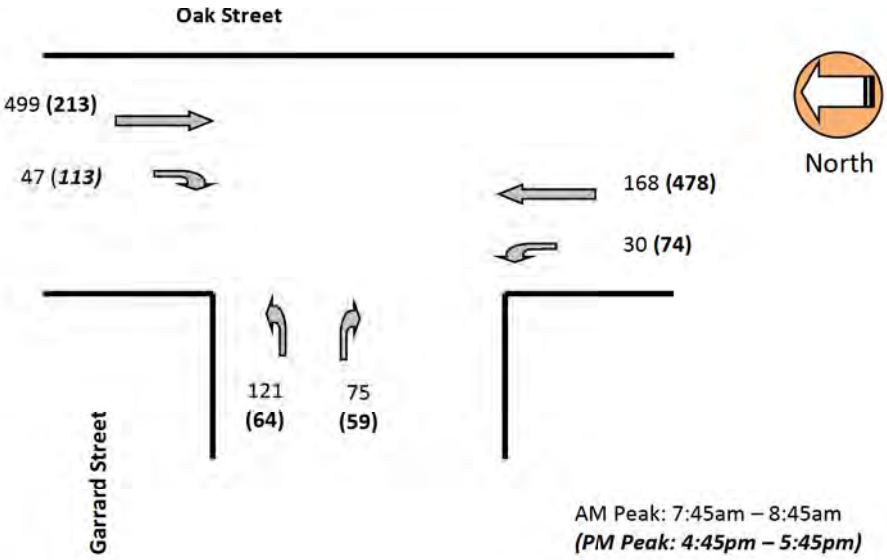


Figure 4 **Post Development Flows**

5.5 Future Intersection Operating Conditions

The Oak Street intersection was analysed using the SIDRA 6 computer package. This computer package analyses various parameters, with the main descriptors described below:

The **Degree of saturation** (DoS) is defined as the ratio of *demand (arrival) flow* to *capacity.* Degrees of saturation above 1.0 represent oversaturated conditions (demand flows exceed capacity), and degrees of saturation below 1.0 represent undersaturated conditions (demand flows are below capacity).

The movement degree of saturation is the largest degree of saturation for any lane of the movement. The approach degree of saturation is the largest DoS value for any movement (or any lane) in the approach, and the intersection degree of saturation is the largest DoS value for any approach. For short lanes, a degree of saturation, DoS = 1.0 means that the average back of queue equals the available short lane storage length, and possibly there is an excess flow in the adjacent lane. For a satisfactory solution, a movement degree of saturation is sought to be less than the nominated *practical (target) degree of saturation*.

The SIDRA output includes estimates of **average delay** and the corresponding Levels of Service (LOS) for movements, lanes, approaches and the intersection. Delay to a vehicle is the difference between interrupted and uninterrupted travel times through the intersection. The *average delay* predicted by SIDRA is for all vehicles, queued and unqueued.

The output reports also include the estimated *back of queue*. The ***Percentile Queue*** parameter is used for the percentile queue length value to be included in output reports. The 95th percentile queue length is the value below which 95 per cent of all observed cycle queue lengths fall, or 5 per cent of all queue lengths exceed.

Table 4 Summary of Level of Service Definitions

Level of Service	Degree of Saturation		
	Signals	Roundabouts	Stop/Giveway
A	DoS < 0.60	DoS < 0.60	DoS < 0.60
B	0.60 < DOs < 0.70	0.60 < DoS < 0.70	0.60 < DoS < 0.70
C	0.70 < DoS < 0.90	0.70 < DoS < 0.85	0.70 < DoS < 0.80
D	0.90 < DoS < 0.95	0.85 < DoS < 0.95	0.80 < DoS < 0.90
E	0.95 < DoS < 1.00	0.95 < DoS < 1.00	0.90 < DoS < 1.00
F	DoS > 1.00	DoS > 1.00	DoS > 1.00

The intersection was analysed using the traffic flows summarised in Figure 4 and the results are summarised in Table 5. The analysis included a review of future conditions assuming Oak Street traffic flows increased by 30%.

Table 5 Summary of Future Intersection Operating Conditions- Garrard St/ Oak St

Scenario	Approach	AM Peak Hour			PM Peak Hour		
		DoS	Ave Delay (s)	95%ile Queue (m)	DoS	Ave Delay (s)	95%ile Queue (m)
2014 base	South (Oak Street)	0.107	0.8	0	0.299	0.8	0
	North (Oak Street)	0.269	0.5	1	0.120	2.8	4
	West (Garrard Street)	0.214	11.8	6	0.205	14.8	5
Future base	South (Oak Street)	0.132	0.6	0	0.376	0.6	0
	North (Oak Street)	0.350	0.6	1	0.150	2.7	4
	West (Garrard Street)	0.321	15.1	9	0.317	20.4	8

The increase in traffic generation represents an increase of 805vpd over that estimated in the Masterplan, and equates to an increase of 1-2 cars per minute during the peak hour at some of the external access points.

Analysis of the Garrard Street/ Oak Street intersection indicates that queues and delays will not be excessive and that the intersection will continue to operate satisfactorily. Further analysis assuming an increase of 30% in passing traffic (on Oak Street) during the peak hours results in a modest increase in the intersection degree of saturation to 0.350 in the AM and 0.376 in the PM peak hour.

The increase in traffic volumes at the other Oak Street intersections will be less than modelled at Garrard Street, and, therefore they are not expected to adversely affect operating conditions at those locations, i.e. Strickland Road or Galada Avenue.

The increase in traffic flows using Willam Street-Gibson Avenue of about 82vph, equates to a little over 1 car per minute and is not expected to adversely affect conditions at the signalised Brunswick Road/ Gibson Avenue intersection.

5.6 Internal Road Network

The additional traffic flows of 2,316vpd, or 232vph in the peaks, will be distributed between the Galada Avenue access points and to a lesser extent the northern freeway apartment's access onto Cade Way. The increase of 232vph can readily be accommodated on Galada Avenue where daily flows are expected to be between 1,500-2,000vpd at the northern and southern ends of the road.

The volume of traffic that each access from the precinct will carry depends on the number of parking spaces that each access will serve. Conservatively assuming that 80% of all traffic generated by the freeway apartments precinct is served by 2 access points onto Galada Avenue, then each access would be expected to carry about 1,098vpd, or 110vph (2-way) during peak periods.

The intersections with Galada Avenue can readily carry 110vph during peak periods without long queues or delays.

It is of note that the original work undertaken by Cardno Grogan Richards expected that Galada would function as a collector road catering for up to 3,000vpd.

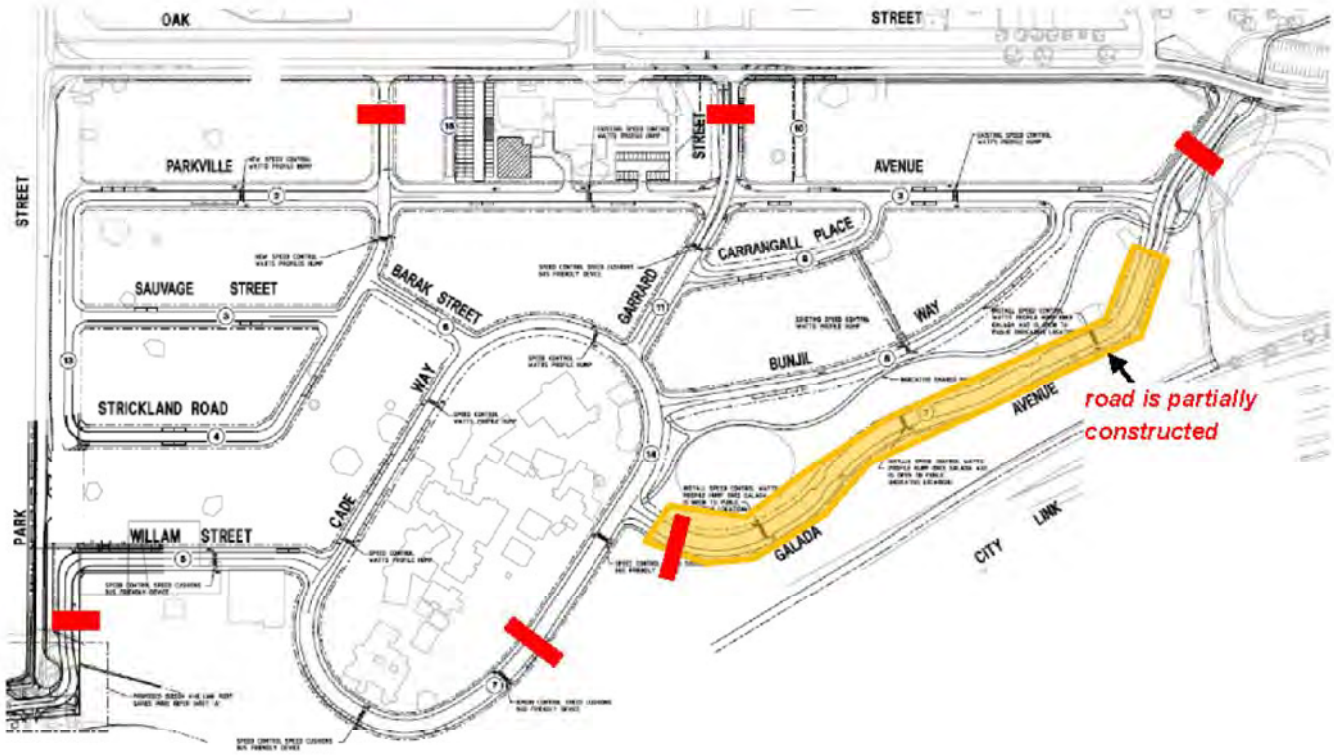
6 Conclusions

- Analysis of traffic flows associated with additional development associated with future stages is expected to result in a total generation of 6,916vpd within Parkville Gardens. The increase in traffic flows equates to 831vpd above that envisaged in the Masterplan.
- The increase in traffic at each of the external access roads equates to 1 -2 vehicles per minute over 2014 levels and is able to be accommodated without adversely affecting passing traffic flows.
- The increase in traffic flows at each of the internal access points to the freeway apartments is well within the capacity of the local road network to accommodate them.
- The estimated flows on Galada Avenue are within the capacity of the collector road.

Appendix 1:

Traffic Survey Sheets

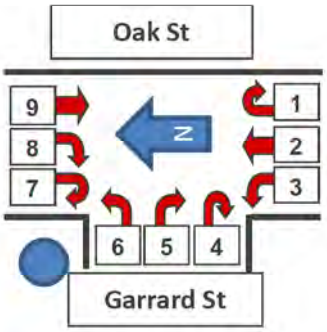
Survey Map 1 Location of Counters



nationwide TRAFFIC SURVEYS

Client: Chris Maragos
Job Name: Park Ville Traffic Survey
Job Number: 3766
Location: Oak St and Garrard St
Date: Thu 23-10-2014
Map Ref: 029 C10
Weather: Overcast

Time: 7-9am and 4-6pm



Absolute Value		VEHICLE MOVEMENTS									Total Hourly
TIME		1	2	3	4	5	6	7	8	9	
7:00	7:15	0	28	3	0	7	5	0	7	74	
7:15	7:30	0	33	2	0	6	7	0	9	88	
7:30	7:45	0	38	0	0	9	13	0	4	106	
7:45	8:00	0	46	4	0	5	15	0	6	145	660
8:00	8:15	0	40	7	0	18	20	0	11	131	763
8:15	8:30	0	41	4	0	8	19	0	9	112	811
8:30	8:45	0	41	6	0	8	14	0	7	111	828
8:45	9:00	0	46	4	0	8	13	0	7	99	784
16:00	16:15	0	93	14	0	8	4	0	6	43	
16:15	16:30	0	113	18	0	7	8	0	7	47	
16:30	16:45	0	97	18	0	5	7	0	12	53	
16:45	17:00	0	120	18	0	14	8	0	11	50	781
17:00	17:15	0	124	12	0	5	15	0	27	65	861
17:15	17:30	0	115	9	0	7	9	0	16	60	877
17:30	17:45	0	119	7	0	11	10	0	19	38	889
17:45	18:00	0	94	7	0	4	6	0	17	47	843

	Approach					
	Oak St north		Oak St south		Garrard St	
	t	rt	t	lt	lt	rt
AM	499	33	168	21	68	39
PM	213	73	478	46	42	37

Oak Street

daily	8744	peak hour factor
am peak	768	9.0%
pm peak	806	

Automatic Traffic Count Summary Sheet

[for tube counters]

Street Name :	Strickland Rd	Location :	Btw Parkville Ave and Oak St
Suburb/Locality :	Parkville	Start Date :	Friday 17 October 14
		Finsh Date :	Thursday 23 October 14
Site ID Number :	3766_01	Speed Zone :	50
Prepared By :	Counters Plus	Road Classification :	
Date :	Tue 28 Oct 14		
File Name :	C:\Users\Chris\Documents\2014 Jobs\CM14019 Parkville\in\surveys\Parkville Survey Results\Parkville Survey Results\CM 3766_01 Strickland Rd Btw Parkville Ave and Oak St.xlsm		

			Direction of Travel		
			Two-Way	Eastbound	Westbound
Traffic Volume :		Week Days Only	843	392	451
[Vehicles/Day]		7 Day Average	818	383	435
Peak Hour	**AM**	8:00	68	41	27
Volume:	**PM**	17:00	77	29	48
Speeds :		85th Percentile	40	38	40
[Km/Hr]		Average	32.6	31.4	33.7
Classification % :		Class 1*	97.4%	98.1%	96.9%

Notes : (Observations)

* Class 1 - Short Vehicles up to 5.5m

Automatic Traffic Counts - Site Data

Site No:	3766_01
Date:	Friday 17 October 14
Start Time:	00:00
Prepared by:	Counters Plus
Road:	Strickland Rd
Suburb/Locality:	Parkville
Location : Btw Parkville Ave and Oak St	
Counter No:	
Map Ref:	029 C10
Comments:	

North Point

Automatic Traffic Count Summary Sheet

[for tube counters]

Street Name :	Garrard St	Location :	Btw Parkville Ave and Oak St
Suburb/Locality :	Parkville	Start Date :	Friday 17 October 14
		Finsh Date :	Thursday 23 October 14
Site ID Number :	3766_02	Speed Zone :	50
Prepared By :	Counters Plus	Road Classification :	
Date :	Tue 28 Oct 14		
File Name :	C:\Users\Chris\Documents\2014 Jobs\CM14019 Parkville\in\surveys\Parkville Survey Results\Parkville Survey Results\CM 3766_02 Garrard St, Btw Parkville Ave and Oak St.xlsm		

			Direction of Travel		
			Two-Way	Eastbound	Westbound
Traffic Volume :		Week Days Only	1,884	1,012	872
[Vehicles/Day]		7 Day Average	1,703	925	778
Peak Hour	**AM**	8:00	159	105	54
Volume:	**PM**	17:00	209	95	115
Speeds :		85th Percentile	39	40	38
[Km/Hr]		Average	32.3	32.9	31.6
Classification % :		Class 1*	93.9%	96.3%	91.1%

Notes : (Observations)

* Class 1 - Short Vehicles up to 5.5m

Automatic Traffic Counts - Site Data

Site No:	3766_02
Date:	Friday 17 October 14
Start Time:	00:00
Prepared by:	Counters Plus
Road:	Garrard St
Suburb/Locality:	Parkville
Location : Btw Parkville Ave and Oak St	
Counter No:	
Map Ref:	029 C11
Comments:	

North Point

Automatic Traffic Count Summary Sheet

[for tube counters]

Street Name :	Galada Ave	Location :	Btw Parkville Ave and Oak St
Suburb/Locality :	Parkville	Start Date :	Friday 17 October 14
		Finsh Date :	Thursday 23 October 14
Site ID Number :	3766_03	Speed Zone :	50
Prepared By :	Counters Plus	Road Classification :	
Date :	Tue 28 Oct 14		
File Name :	C:\Users\Chris\Documents\2014 Jobs\CM14019 Parkville\in\surveys\Parkville Survey Results\Parkville Survey Results\CM 3766_03 Galada Ave Btw Parkville Ave and Oak St.xlsm		

			Direction of Travel		
			Two-Way	Eastbound	Westbound
Traffic Volume :		Week Days Only	561	266	295
[Vehicles/Day]		7 Day Average	527	247	280
Peak Hour	**AM**	6:00	49	11	38
Volume:	**PM**	15:00	47	37	10
Speeds :		85th Percentile	39	38	40
[Km/Hr]		Average	32.3	31.7	32.9
Classification % :		Class 1*	92.7%	92.9%	92.5%

Notes : (Observations)

* Class 1 - Short Vehicles up to 5.5m

Automatic Traffic Counts - Site Data

Site No:	3766_03
Date:	Friday 17 October 14
Start Time:	00:00
Prepared by:	Counters Plus
Road:	Galada Ave
Suburb/Locality:	Parkville
Location : Btw Parkville Ave and Oak St	
Counter No:	
Map Ref:	029 C11
Comments:	

North Point

Automatic Traffic Count Summary Sheet

[for tube counters]

Street Name :	Galada Ave	Location :	Just South of Cade Way
Suburb/Locality :	Parkville	Start Date :	Friday 17 October 14
		Finsh Date :	Thursday 23 October 14
Site ID Number :	3766_04	Speed Zone :	50
Prepared By :	Counters Plus	Road Classification :	
Date :	Tue 28 Oct 14		
File Name :	C:\Users\Chris\Documents\2014 Jobs\CM14019 Parkville\in\surveys\Parkville Survey Results\Parkville Survey Results\CM 3766_04 Galada Ave South of Cade Way.xlsm		

			Direction of Travel		
			Two-Way	Northbound	Southbound
Traffic Volume :		Week Days Only	239	119	120
[Vehicles/Day]		7 Day Average	246	123	123
Peak Hour	**AM**	11:00	18	9	9
Volume:	**PM**	19:00	17	7	11
Speeds :		85th Percentile	29	29	29
[Km/Hr]		Average	23.4	23.8	23.0
Classification % :		Class 1*	96.3%	96.3%	96.3%

Notes : (Observations)

* Class 1 - Short Vehicles up to 5.5m

Automatic Traffic Counts - Site Data

Site No:	3766_04
Date:	Friday 17 October 14
Start Time:	01:00
Prepared by:	Counters Plus
Road:	Galada Ave
Suburb/Locality:	Parkville
Location : Just South of Cade Way	
Counter No:	
Map Ref:	29 C11
Comments:	

North Point

Automatic Traffic Count Summary Sheet

[for tube counters]

Street Name :	Cade Way	Location :	West of Galada Ave
Suburb/Locality :	Parkville	Start Date :	Friday 17 October 14
		Finsh Date :	Thursday 23 October 14
Site ID Number :	3766_05	Speed Zone :	50
Prepared By :	Counters Plus	Road Classification :	
Date :	Tue 28 Oct 14		
File Name :	C:\Users\Chris\Documents\2014 Jobs\CM14019 Parkville\in\surveys\Parkville Survey Results\Parkville Survey Results\CM 3766_05 Cade Way West of Galada Ave.xlsm		

			Direction of Travel		
			Two-Way	Eastbound	Westbound
Traffic Volume :		Week Days Only	582	237	345
[Vehicles/Day]		7 Day Average	568	234	334
Peak Hour	AM	8:00	40	23	18
Volume:	PM	17:00	56	17	39
Speeds :		85th Percentile	36	35	36
[Km/Hr]		Average	29.0	28.5	29.4
Classification % :		Class 1*	92.9%	93.7%	92.4%

Notes : (Observations)

* Class 1 - Short Vehicles up to 5.5m

Automatic Traffic Counts - Site Data

Site No:	3766_05
Date:	Friday 17 October 14
Start Time:	00:00
Prepared by:	Counters Plus
Road:	Cade Way
Suburb/Locality:	Parkville
Location : West of Galada Ave	
Counter No:	
Map Ref:	C029 B10
Comments:	

North Point

Automatic Traffic Count Summary Sheet

[for tube counters]

Street Name :	William St	Location :	West of William St
Suburb/Locality :	Parkville	Start Date :	Friday 17 October 14
		Finsh Date :	Thursday 23 October 14
Site ID Number :	3766_06	Speed Zone :	50
Prepared By :	Counters Plus	Road Classification :	
Date :	Tue 28 Oct 14		
File Name :	C:\Users\Chris\Documents\2014 Jobs\CM14019 Parkville\in\surveys\Parkville Survey Results\Parkville Survey Results\CM 3766_06 William St West of William St.xlsm		

			Direction of Travel		
			Two-Way	Eastbound	Westbound
Traffic Volume :		Week Days Only	1,457	677	780
[Vehicles/Day]		7 Day Average	1,351	641	710
Peak Hour	**AM**	8:00	107	62	45
Volume:	**PM**	17:00	168	55	114
Speeds :		85th Percentile	34	36	32
[Km/Hr]		Average	29.5	31.5	27.8
Classification % :		Class 1*	95.8%	95.1%	96.4%
Notes : (Observations)					
* Class 1 - Short Vehicles up to 5.5m					

Automatic Traffic Counts - Site Data

Site No:	3766_06
Date:	Friday 17 October 14
Start Time:	00:00
Prepared by:	Counters Plus
Road:	William St
Suburb/Locality:	Parkville
Location : West of William St	
Counter No:	
Map Ref:	029 B10
Comments:	

North Point

Appendix 2:

Freeway Apartments Survey


Stage 1,2,3 - 110 Car Spaces
Stage 2 - 19 Car Spaces
Stage 2,3 - 29 Car Spaces
Stage 5 - 57 Carspaces

CM Parkville Apartment Survey

Project: Parkville apartments
Location: Cade Way, Parkville
Date: 16-May
Time: 7am to 7pm
No of aprtments 215

Stage		1, 2 and 3		2		2 and 3		5			Hourly		
Time		IN	OUT	In	Out	In	Out	In	Out	Rate	Overall	in	out
07:00	07:15	0	2	0	0	0	1	0	1				
07:15	07:30	0	0	0	0	0	0	1	0				
07:30	07:45	0	1	0	0	0	0	1	2				
07:45	08:00	1	1	0	0	0	0	2	2		15		
08:00	08:15	2	3	0	0	0	0	1	2		19		
08:15	08:30	0	2	0	0	0	0	0	1		21		
08:30	08:45	2	1	0	0	0	0	1	2		23		
08:45	09:00	3	2	0	0	0	0	5	1	0.13	28	14	14
09:00	09:15	2	0	0	0	0	3	1	0		26		
09:15	09:30	3	1	1	1	2	3	4	3		41		
09:30	09:45	1	0	0	0	0	1	0	1		38		
09:45	10:00	0	0	0	1	0	1	1	1	0.14	31	15	16
10:00	10:15	0	3	0	0	1	1	2	4		36		
10:15	10:30	2	3	0	0	0	0	1	3		27		
10:30	10:45	0	2	0	1	1	0	1	2		31		
10:45	11:00	2	4	0	1	0	0	2	5		41		
11:00	11:15	1	3	0	0	1	0	2	2		39		
11:15	11:30	0	3	0	1	2	1	1	2		40		
11:30	11:45	0	1	0	1	2	0	0	0		37		
11:45	12:00	2	0	0	0	1	0	4	0		30		
12:00	12:15	0	3	0	1	0	1	1	4		31		
12:15	12:30	1	2	1	1	0	0	1	3		30		
12:30	12:45	0	4	1	1	0	0	0	5		37		
12:45	13:00	0	1	0	0	1	2	0	1		35		
13:00	13:15	1	1	0	0	0	1	1	0		29		
13:15	13:30	1	2	1	0	0	0	2	3		29		
13:30	13:45	2	0	0	1	1	0	2	2		26		
13:45	14:00	0	4	0	0	0	1	1	2		29		
14:00	14:15	2	3	0	0	1	0	2	4		37		
14:15	14:30	3	0	0	0	1	0	2	2		36		
14:30	14:45	2	5	0	1	0	0	3	8	0.22	47	17	30
14:45	15:00	0	1	2	0	0	0	2	2		46		
15:00	15:15	3	0	0	0	0	0	2	0		39		
15:15	15:30	0	1	0	1	0	0	0	2		35		
15:30	15:45	0	1	1	0	0	0	0	0		18		
15:45	16:00	1	1	0	0	1	0	2	0		16		
16:00	16:15	2	2	0	1	1	0	1	3		21		
16:15	16:30	0	0	0	1	0	0	0	0		18		
16:30	16:45	3	2	0	0	0	0	4	2		27		
16:45	17:00	3	2	0	0	0	0	2	3		32		
17:00	17:15	2	2	0	0	0	0	2	1		29		
17:15	17:30	4	1	1	0	1	1	3	0		39		
17:30	17:45	3	0	2	1	0	1	4	0		39		
17:45	18:00	1	0	1	0	1	0	0	0		32		
18:00	18:15	0	2	1	0	0	0	1	1	0.14	30	23	7
18:15	18:30	1	2	1	0	2	1	1	3	0.14	30	19	11
18:30	18:45	2	1	0	0	0	0	3	0		25		
18:45	19:00	0	0	0	0	0	0	1	3		26		

CM Parkville Apartment Survey

Project: Parkville apartments
Location: Cade Way, Parkville
Date: 14-May
Time: 7am to 7pm
No of aprtments 215 apartments

Stage		1, 2 and 3		2		2 and 3		5			Hourly		
Time		IN	OUT	In	Out	In	Out	In	Out	Rate	Overall	in	out
07:00	07:15	0	0	0	0	0	0	3	0				
07:15	07:30	0	0	0	0	1	0	0	5				
07:30	07:45	0	0	0	0	1	0	0	5				
07:45	08:00	0	2	0	0	0	1	3	4		25		
08:00	08:15	2	3	1	0	2	0	0	8		38		
08:15	08:30	1	4	0	0	2	0	0	1		40		
08:30	08:45	0	2	0	0	0	0	0	5	0.19	41	11	30
08:45	09:00	0	3	1	2	0	1	2	4	0.20	44	11	33
09:00	09:15	0	3	0	0	1	0	2	5		39		
09:15	09:30	1	3	1	0	0	0	2	1		39		
09:30	09:45	1	0	0	0	0	0	0	2		35		
09:45	10:00	0	0	0	0	0	1	0	3		26		
10:00	10:15	0	0	2	0	0	2	1	1		21		
10:15	10:30	1	0	0	0	0	0	0	4		18		
10:30	10:45	0	1	0	0	0	0	0	0		16		
10:45	11:00	0	0	0	0	0	2	2	4		20		
11:00	11:15	0	0	0	0	0	0	0	1		15		
11:15	11:30	0	1	1	2	0	0	2	4		20		
11:30	11:45	1	0	0	0	1	0	0	0		21		
11:45	12:00	1	1	0	0	0	0	1	1		17		
12:00	12:15	0	1	0	0	0	0	2	1		20		
12:15	12:30	0	2	0	1	0	0	1	2		16		
12:30	12:45	1	0	0	0	0	0	1	2		18		
12:45	13:00	0	1	1	0	1	0	5	2		24		
13:00	13:15	1	1	0	0	1	0	2	3		28		
13:15	13:30	1	0	0	1	2	1	1	1		29		
13:30	13:45	0	1	1	0	1	0	1	0		29		
13:45	14:00	2	1	0	0	0	1	0	1		24		
14:00	14:15	1	1	0	0	0	1	0	4		23		
14:15	14:30	1	1	1	0	0	0	0	4		23		
14:30	14:45	1	0	0	0	0	0	1	0		21		
14:45	15:00	1	0	1	0	0	0	1	2		21		
15:00	15:15	1	1	0	0	0	1	1	1		19		
15:15	15:30	2	0	0	0	0	0	2	3		19		
15:30	15:45	0	0	0	1	0	1	1	2		22		
15:45	16:00	1	0	0	0	2	0	2	2		24		
16:00	16:15	0	0	0	0	0	1	1	0		21		
16:15	16:30	2	1	1	0	0	0	2	2		22		
16:30	16:45	1	0	0	0	2	2	3	1		26		
16:45	17:00	2	3	2	0	0	0	1	3		30		
17:00	17:15	0	1	1	0	0	1	2	2		35		
17:15	17:30	0	0	0	0	1	0	2	7		37		
17:30	17:45	3	1	0	0	0	3	2	0	0.17	37	16	21
17:45	18:00	1	0	0	1	0	1	5	1		35		
18:00	18:15	2	0	0	1	2	0	4	4	0.19	41	22	19
18:15	18:30	2	2	0	0	1	0	2	2		40		
18:30	18:45	0	0	0	1	2	0	3	2	0.18	39	24	15
18:45	19:00	5	2	0	0	0	0	1	1		39		

SJB Urban

Village Park Consortium

VPC Apartments Precinct

Stages 9-12

Urban Design Response

Revision 16 // November 2016

Project
VPC Apartments Precinct
Village Park Consortium

Ref #66778
Date issued: November 15, 2016
Version: 16

Contact Details
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Introduction: Context & Approach

1.1 Introduction

This Urban Design Response (UDR) sets out the urban design background and directions for the Parkville Gardens Freeway Apartments Precinct, Stages 9-12.

The Apartments Precinct has been developed over several years, with earlier stages delivered in a general sequence from north to south. Stages 9-12 form the final planned developments in this Precinct, located between City Link and the established residential area of the Parkville Gardens Freeway Apartments Precinct, to the north of the established wetlands area and adjacent to the Melbourne Gateway.

The development to date has been guided by the established Incorporated Plan, and associated Siting and Design Guidelines (SDG). The current Incorporated Document (*The Games Village Project, Parkville*) was prepared in September 2006 (previous version dated October 2003). The current SDG document was prepared in 2009, with minor updates dated 14 January 2011. The Master Plan was approved in January 2004.

Recently, the development consortium, Village Park Consortium (VPC) has identified opportunities to refine and enhance the urban design outcomes proposed for the last stages, from those envisaged in the abovementioned documents.

This UDR sets out the principles and parameters of this new approach, and explains the proposed outcomes and benefits.

1.2 Executive Summary

This Urban Design Response proposes a refined, contemporary design outcome for Stages 9-12, which delivers a more sophisticated, complex and responsive arrangement of built form and open spaces, within the parameters established for the overall Apartments Precincts by earlier guiding documents.

The key outcomes set out in this UDR are summarised as follows:

- **Same average building height (habitable levels)** across all apartment buildings at the Parkville Gardens Freeway Apartments Precinct, as envisaged in the exiting SDG (2009), and lower average height than that sought by the Master Plan (2004). The Master Plan (2004) envisaged an average height of 6 habitable storeys, the SDG (2009) showed average 4.94 habitable storeys. This UDR also envisages an average of 4.94 habitable storeys cross all apartment buildings;
- **Increased total open space** across the Parkville Gardens Freeway Apartments Precinct, to that envisaged by the Master Plan (2004), by some 3,757 additional square metres, and enhanced local open space adjoining Stage 12;
- **No net increase in overshadowing** to the adjacent wetlands to the south of the Apartments Precinct;
- **Lower-height, more recessive built form at closest / most sensitive location** relative to nearby established houses;
- **Moderate increase in residential yield** relative to the original SDG, of 117 apartments.
- **Increased maximum preferred height of Stage 11** to 19 habitable storeys, rather than 11 habitable storeys;
- **Potential to leverage other strategic opportunities**, such as improved public transport services, increased activation and increased social housing delivery.

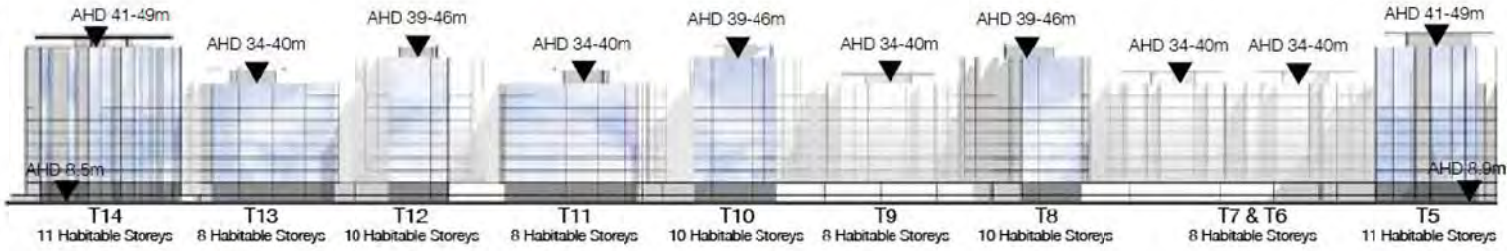
1.3 Vision

Building on the above-mentioned context, the team has established a clear vision, as articulated in this UDF, to focus the final four stages on achieving:

- *Equivalent total apartment development extent as originally envisaged in the SDG and Incorporated Document;*
- *A more sophisticated, contemporary and integrated urban design outcome;*
- *A distinctive, visually striking cluster of buildings that are positioned for an optimal balance of amenity outcomes;*
- *Changed form and shape of buildings, with some higher, and some lower built form than the current framework;*
- *A dynamic, sculptural response to the freeway setting and Melbourne Gateway;*
- *Formal configuration of buildings to address the complex interfaces of freeway to the west, and residential area and park to the east;*
- *Arrangement of buildings to optimise protection from freeway noise;*
- *Increased public benefit, in the form of improved public transport accessibility, enhanced public open space, and State Government potential for extra affordable/ community housing.*

Parkville Gardens Freeway Apartments Building Layout Plan
Source: Siting and Design Guidelines 2009 (minor updates 2011), page 20

Parkville Gardens Freeway Apartments Building Layout Elevation
Source: Siting and Design Guidelines 2009 (minor updates 2011), page 20



1.4 Current Urban Design Parameters

Stages 9-12 are defined by various urban design parameters, as drawn from the current *Parkville Gardens Freeway Apartments Siting and Design Guidelines* (SDG) and the Incorporated Plan.

Importantly, the original urban design approach for the Apartments Precinct comprised a generally continuous 'wall' of built form that curved along the freeway edge, as an urban edge between the freeway and low-scale housing within the Parkville Gardens Freeway Apartments Precinct site. This approach also incorporated 'bookend' gestures, featuring more prominent built form, at the northern and southern end of the Apartments Precinct.

This UDR articulates a new formal approach to Stages 10-12, to form a prominent yet context-responsive 'urban marker'.

Building Types and Alignments

The current Siting and Design Guidelines describe four (4) different apartment building types, as follows:

- Tower type 01: *The tower apartments have been designed as a series of 'fingers' projecting towards the eastern parkland.*
- Tower type 02
- Street-edge apartments
- Spine apartments

The Tower apartments are aligned perpendicular to the freeway and park, with a central core and corridor and apartments to the north and south sides.

The street-edge apartments are lower-scale types fronting the parkland and forming an edge to the car parking podium. The spine apartments form a linear built form, between Tower buildings, along the freeway interface.

This UDR and the proposed revised SDG introduces a new type; Tower Type 3, applicable to Stages 10-12, as explained below.

Building Heights

For the southern part of the Apartments Precinct (Stages 9-12), the SDG provides for buildings heights ranging from 8 to 11 Habitable Storeys, with specific guidance as follows:

- Stage 9: 8 Habitable Storeys (Tower Type 02)
- Stage 10: 10 Habitable Storeys (Tower Type 01)
- Stage 11: 8 Habitable Storeys (Tower Type 02)
- Stage 12: 11 Habitable Storeys (Tower Type 02)

As the SDG illustrate, the building heights are quite consistent, and create relatively 'squat' building forms at this important southern termination of the linear precinct.

Further to the above, the Master Plan (2004) and Incorporated Document (2003 and updated 2006) contains a Design Objective for apartments generally, ***to achieve an average apartment height of approximately six habitable storeys***. This is calculated by measurement of the length of each building multiplied by its height.

The framework articulated in the current SDG achieves an average height across all Parkville Gardens Freeway Apartments Precinct buildings of 4.94 habitable storeys.

This UDR proposes some increase in building heights, but with enhanced shaping of building to address sensitive interfaces and formal and spatial qualities, with a more sophisticated and contemporary urban design outcome. Within this proposed response, the overall average height will remain at 4.94 habitable storeys.

Document	Average height (habitable storeys)
Master Plan (2004)	6.00
Siting and Design Guidelines (2006 and 2009)	4.94
Proposed SDG and this UDR (2015)	4.94

Open Space

The *Siting and Design Guidelines* indicate an open space at the southern end of the development area of the Apartment Precincts, providing a 'green link' between the existing park and the wetlands area.

This initiative is retained and expanded under the new response, described in Part 2 below.

Public Realm Interfaces

The built form response presents an above-ground podium address to the freeway interface, with an access way between this built edge and the freeway acoustic wall. At the street/park interface, lower-level apartments are generally positioned at the frontage of the car parking podium, to create an active residential edge.

The new approach articulated in this UDR retains a similar approach of activated podium facing Galada Avenue and the park.

Completed development at Village Park, looking toward the subject land for Stages 10/11/12

1.5 Urban Design Analysis/Assessment of the Current Response

Upon review, the current respose under the *Siting and Design Guidelines* presents a number of urban design considerations, issues and opportunities for potential improvement, as follows:

Limited Variation in Building Heights
The Stage 9-12 buildings range in height from 8-11 storeys (habitable storeys above podium), presenting a consistent series of mid-rise buildings with relatively 'squat' proportions. This is in the context of preceding Stages, also with similar heights as follows:

- Stage 4 – 11 HS (permit approved, currently under amendment and marketing)
- Stage 5 – 5 HS (completed)
- Stage 6 – 7 HS (completed)
- Stage 7 – 11HS (under construction)
- Stage 8 – 9 HS (permit approved, construction not yet commenced).

As illustrated in the Building Heights Diagram in the SDG (2009) (page 20), while there is a subtle undulation in building heights across the Stages, the forms are quite consistent in height.

While this is not necessarily considered an urban design issue in itself, it creates an impression of a constrained or constricted development setting, in the context of the freeway and Melbourne Gateway. That is, it appears 'cut off' at a moderate height.

There is opportunity for a more varied, legible and defined urban form condition across the Stages, as discussed below. Also, some parts of the Precinct present opportunities for increased built form scale (with greater distance from the park and existing houses), with potential for lower heights in more sensitive locations (closer to existing houses).

This outcome can be achieved within the same average apartment building height (4.94 habitable storeys) as the current SDG Framework provides for, which is considerably below the average six (6) habitable storeys that the Incorporated Document and Master Plan seeks to achieve for apartment buildings.

Issue: Rectilinear, Rigid Building Forms
The built form character of the existing buildings, and those indicated in the SDG for Stages 9-12, is generally rectilinear, with buildings regularly space in a linear sequence. While this character is not an issue in itself, there is potential to create a more 'fluid', dynamic and sculptural built form character in response to the freeway, park and wetland context.

Opportunity: At the southern end (Stages 9-12), the Precinct becomes wider as it 'turns the corner' to interface with the Wetlands, presenting opportunities for a more dynamic arrangement of forms which are more 'fluid' in form and character.

Issue: Lack of a 'Bookmark' Device at the Southern End of the Precinct
The consistent heights prevent the presence of a strong visual marker or 'anchor' form, at the southern end of the Apartments Precinct.

The linear Apartment Precinct is most commonly experienced from the freeway environment at significant speed. Therefore its legibility would be enhanced by forms that are varied, and that define the Precinct as an urban setting, with defined edges.

Further, the generally consistent rectilinear forms and materials of the developed Stages adds to this sense of consistency, or lack of variation, even though each building has a distinct design character when viewed from close-range.

Opportunity: There is opportunity for the southern part of the Precinct to be developed as an urban 'marker', close to the prominent Melbourne Gateway, to define the extent of the Apartment Precinct as perceived from the freeway environment.

Issue: Limited spacing between buildings
The current SDG reflects quite limited spacing between buildings, in the southern part of the Precinct, with forms becoming wider closer to the freeway, and effectively 'joining up' in some locations. This serves to effectively create a continuous wall of built form, with generally consistent heights as discussed above.

Opportunity: There is potential for buildings to be positioned in a more varied and 'nuanced' arrangement, to optimise the spacing between adjacent buildings, and to enhance outlook and access to daylight, as well as the visual impression of the buildings as seen from the freeway and within the Precinct.

1.6 Physical Context

The Apartment Precinct is located in a highly prominent location, particularly in terms of visibility from the CityLink freeway corridor for vehicles travelling between the CBD and Melbourne Airport (and beyond).

The Precinct adjoins the landmark Melbourne Gateway, a dynamic sculptural intervention within the freeway corridor. The previously proposed East-West Link was planned to intersect with the CityLink freeway at this location, and would have extended cross the existing Wetlands area.

The relatively recent Travencore development, located close to the Apartment Precincts on the west side of City Link, extends up to some 27 storeys, forming a reference point for significant recent development along this freeway corridor north of the Melbourne CBD.

There are a number of other recent higher-density residential developments in the local area.

The southern end of the Apartments Precinct contains a major services easement (11m wide) running across the site, between the proposed Stages 9 and 10, as shown in the plans below. This easement cannot accommodate development, but would be used for vehicle access into podium parking areas, for example.

The defining characteristic of this relatively narrow, linear development precinct is the disparate contextual conditions of the freeway environment to the west, and the low-scale residential and park setting to the east. This presents a design challenge for establishing built form which effectively mediates this marked 'difference' in the immediate context.

1.7 Strategic Context

While it is not the purpose of this UDR to provide a comprehensive review of the applicable strategic/statutory planning context affecting this project, the following excerpts from key documents provide further strategic foundations for this UDR and the proposed new approach to built form.

The Siting and Design Guidelines outline:

Design Objective 10.3

- *'To design apartment buildings that respond to their context taking into account sightlines external to the site.'*
- *'To promote high quality apartment developments that make a positive contribution to the built form of the area, provide architecturally interesting façades and provide a high level of internal amenity for residents.'*
- *'To achieve an average apartment height of approximately six habitable storeys.'*
- *'To provide a diversity of architectural expression in the design of the apartment buildings.'*

Design Objective 10.4

- *'To create an apartment layout along the western boundary of the site that is dynamic, articulated, curvilinear and highly modulated and enhances the existing Melbourne Gateway.'*

2

Current Context and Approach

This Part of the Urban Design Response describes a new proposed approach to development for Stages 9-12, which responds to the urban design issues and opportunities described above.

2.1 Urban Design opportunities

A review of the *Siting and Design Guidelines* for Stages 9-12 presents a number of urban design opportunities for future development in the Apartments Precinct, as follows:

Achieving more fluid, dynamic building forms
Future buildings in the Precinct can adopt a more 'streamlined', dynamic form and expression in response to the curved and 'fluid' nature of the context (freeway alignment, park and wetlands setting), as distinct from the generally rectilinear existing buildings.

This approach will enhance the urban appearance, visual interest and contextual response, and form a distinctive and unique cluster of buildings in this prominent location.

Responding to the 'high speed' environment of the freeway
Sweeping, dynamic building forms reflect a more responsive address to the high-speed freeway environment, which is characterised by broad curves and fast movement. That is, the perception of the buildings from the freeway environment will be enhanced through more sculptural forms, the views of which change as one moves past around the precinct, with the buildings appearing to overlapping and separate as seen from various locations.

Responding to the low-scale / 'low speed' environment of the park and wetlands
The Precinct also presents opportunities to more effectively respond to the park/wetlands environment through the form and arrangement of buildings. Buildings which are more 'organic' in shape will present a contextual response to this landscape setting, achieving softer edges and more subtle building shapes, as perceived from the park and wetlands area.

Achieving appropriate spacing between buildings and amenity outcomes
Refined building forms and inter-relationships can achieve increased building separation, and building alignments which optimise outlook and daylight access for occupants.

Buildings arranged side-by-side in a line can create good outlook for occupants, but tend to form an impermeable 'wall' of built form. Buildings that are more spaced apart but face each other achieve limited outlook and amenity for residents.

Therefore a 'hybrid' arrangement of partially overlapping and well-spaced curvilinear buildings can achieve an optimal balance of outlook, daylight access and views between buildings.

Capturing key views between buildings
Building forms can be positioned to reinforce the spacing and separation as perceived from the freeway at speed, while also facilitating views from inside the Precinct. The overlapping buildings and curved profiles which taper away from each other, create wider access to views between buildings, and a dynamic sense of building separation as one moves through and around the precinct.

Creating a 'bookend' as a cluster of buildings
The southern part of the Precinct can become a threshold or 'bookend' to the Precinct and wider Parkville Gardens Freeway Apartments Precinct area, and a sculptural urban 'marker' at the south-west corner of the Parkville Gardens Freeway Apartments Precinct and close to the Melbourne Gateway.

This 'marker' is conceived as a cluster of distinctive, inter-related buildings, rather than a single 'landmark' building.

Avoiding increased shadow impacts
A new built form approach in this part of the Apartments Precinct should avoid any increase in shadow impacts to the adjacent Wetlands area to the south of the precinct, when compared to the current SDG built form framework.

This principle presents potential for more sculpted buildings, with height increases in some locations, and reduced building massing in other locations closer to the wetlands, to achieve a net reduction in overshadowing.

Reducing built form mass in proximity to existing houses
While building height is proposed to be increased in some areas closer to the freeway, there is opportunity to reduce the scale and mass of built form close to existing houses and the wetlands within the Parkville Gardens Freeway Apartments Precinct.

This concentration of built form massing away from sensitive interfaces is an essential consideration in planning for increased development outcomes in this established urban context.

Enhanced design quality, visual interest and building performance
It is foreseen that a new urban approach should deliver better buildings and spaces between buildings, more interesting architectural and urban design, and buildings with enhanced sustainability and amenity performance outcomes.

This evolving precinct has allowed the proponent and design teams to develop technical knowledge and test innovative construction techniques over an extended period. At Stages 9-12, the team is focused on a new formal approach, aligned with enhanced amenity and technical performance in buildings.

Enhanced public and communal recreation spaces
The original SDG provided for a 'pocket park' space at the southern 'tip' of the Apartments Precinct. This provision is retained and enhanced in the proposed framework (relative to the existing SDG), forming an important public open space link between the existing park and wetlands areas, as shown below.

At podium level, the proposed curvilinear building footprints present opportunities for similarly dynamic and fluid communal spaces and facilities, including private and shared open spaces and a residents' facilities pavilion in the south-east corner of the podium.

Retained apartment building 'extent' as original SDG
Importantly, the improved urban design outcomes outlined above can be achieved while retaining the same average apartment building height as reflected in the current SDG (4.94 habitable storeys, across all apartment buildings), which is well below the design objective established in the Incorporated Document and Master Plan for achieving an average of six (6) habitable storeys across all apartment buildings.

2.2 Other strategic opportunities

A new approach to built form provisions also presents opportunities for enhanced strategic outcomes and community benefits in the Precinct, as follows, which the project proponent has rigorously pursued:

Optimising local population close to the CBD, public transport, facilities and services
Strategic support for higher-density development in well-located and well-serviced locations is reinforced by *Plan Melbourne*, which builds upon earlier metropolitan strategic planning in *Melbourne 2030* and *Melbourne @ 5million*.

Increased housing provides opportunities for more people to live in established urban areas that benefit from good access to jobs, transport and community and recreation facilities.

The combination of enhanced urban design outcomes and an extent of residential development which is aligned with and responsive to local considerations of capacity, amenity, accessibility, impacts and contextual 'fit', provides for an appropriate development proposition.

The enhanced urban design / built form framework for Stages 9-12 set out below allows the overall development to 'reach;' the extent of development envisaged by the current SDG framework (average 4.94 habitable storeys).

Improved local public transport service and frequency
This UDR establishes a more responsive, sophisticated urban design outcome, for Stages 9-12. This response will achieve the extent of apartment built form envisaged by the existing SDG (2009) of average 4.94 habitable storeys. In turn, this outcome will contribute to achieving the envisaged levels of public transport service and frequency.

Appropriate development densities support the viability, and potential delivery, of effective public transport services. Higher-density development, along with effective public transport services, provides the foundation for encouraging 'mode shift' away from private car use. In this location, it is envisaged that increased bus service frequencies will enhance the potential for sustainable travel and mode-shift.

It is understood that current bus services through this Precinct are in high demand during peak travel times, but run at low frequencies currently. While service improvements are not currently committed or funded by PTV or bus operators, this potential has been identified and discussed with/acknowledged by PTV.

Increased affordable/social housing
Under the provisions of the Project Delivery Agreement that has guided this development to date, there is opportunity for the Department of Health and Human Services to request additional Social/Affordable housing be provided over and above the 200 dwellings already allocated to the project. This will provide the potential for greater social diversity in the precinct and also accommodating more people, from various backgrounds, in a well-located and well-designed residential setting.

Enhanced activation and passive surveillance outcomes
Finally, optimising the local residential population means more people movements, more eyes on the street, and more windows and balconies providing opportunities for passive surveillance of the public realm, and a more vibrant urban area. While these benefits must be balanced against relevant built form considerations (bulk, views, amenity), as well as capacity and infrastructure considerations, an increased local population is seen as advantageous in encouraging social diversity and activity.

A New Approach

3.1 Urban Design Response Plan

This plan (below) describes the proposed Urban Design Response for Stages 9-12 of the Apartments Precinct. The Response comprises the following defining elements:

- Stage 9: rectilinear building form (in relation to neighbouring buildings to the north), located north of the services easement;
- Stages 10-12: integrated, 'pebble-shaped' buildings in plan, forming a cluster of distinctive curvilinear and dynamic buildings;
- Continuous podium (3-4 storeys) containing car parking and edge activation to Galada Avenue and existing park;
- Residents' recreation pavilion, on podium level (south-west);
- Communal and private open spaces on podium level;
- New public open space (enhanced and expanded from that envisaged in the SDG), at the southern 'tip' of the precinct, with edge activation through potential communal/recreation activities.

3.2 Outcomes

The new response+++ presents a number of beneficial urban design outcomes, as follows:

3.2.1 Radial arrangement

The buildings are oriented in a radial arrangement, in which the building 'spines' appear to radiate outwards in sequence towards the south, creating a visual 'fanning' effect as the buildings are viewed from the freeway moving south. This also creates a sense of layering, overlap and 'depth' in the arrangement and inter-relationships of buildings, and creates distinct perceptions of the arrangements as viewed from different locations.

While the buildings are partially curvilinear in plan, they also present clear alignments and inter-relationships, providing a visible structure to their positioning and configuration, alongside the dynamic, sweeping curved facades.

3.2.2 Views between buildings

The spacing between buildings is designed to achieve multiple outcomes:

- Providing glimpses between buildings from the freeway as one moves past at speed, through gaps which are angled towards the driver rather than perpendicular to the freeway;
- Creating a sense of depth and layering/overlap of building forms;
- Views that 'open out' in various directions, from within or around the buildings.

For pedestrians within the Parkville Gardens Freeway Apartments Precinct, views between buildings will be available from a range of locations, and the apparent width of the spacing will change as one moves around the adjoining streets and parkland. Further, the varied orientation of the spacing between buildings will result multiple and overlapping views between buildings from some locations.

3.2.3 Views outwards

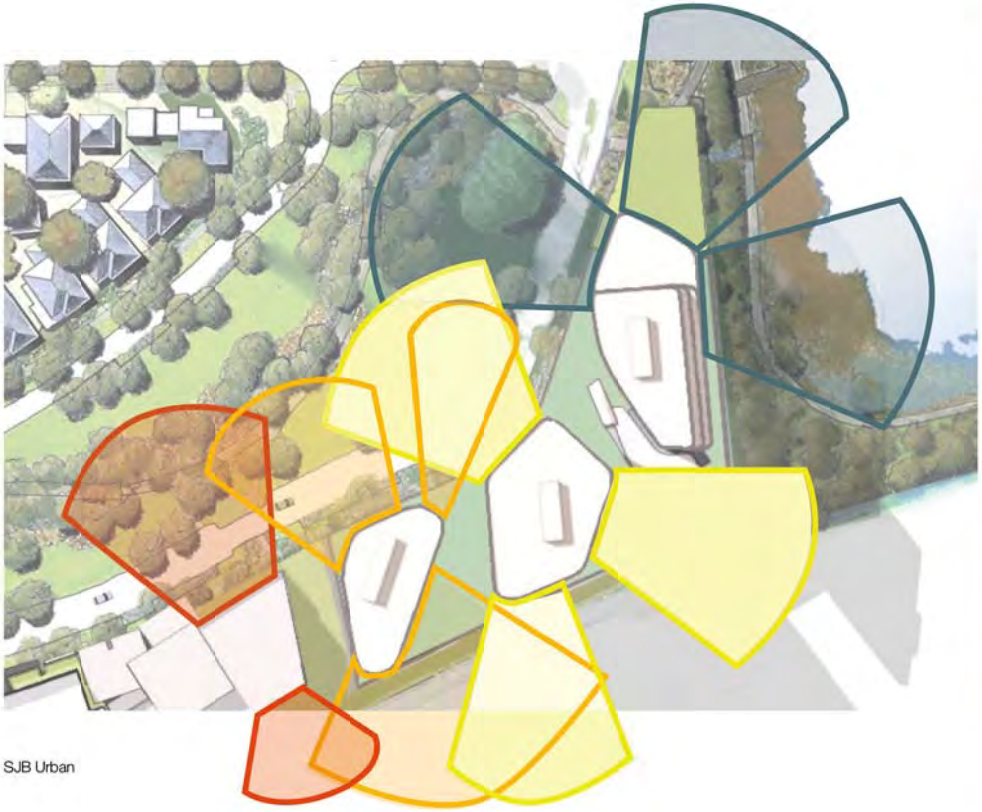
The irregular building footprints and staggered, radial arrangement, coupled with effective internal building

layouts, creates optimal potential for views outwards from the buildings, and access to daylight and sunlight. That is, while the buildings are yet to be fully designed, their internal layouts of apartments, and orientation of living spaces and private balconies, will be configured to avoid direct inter-looking between adjacent buildings, as shown in the diagram below.

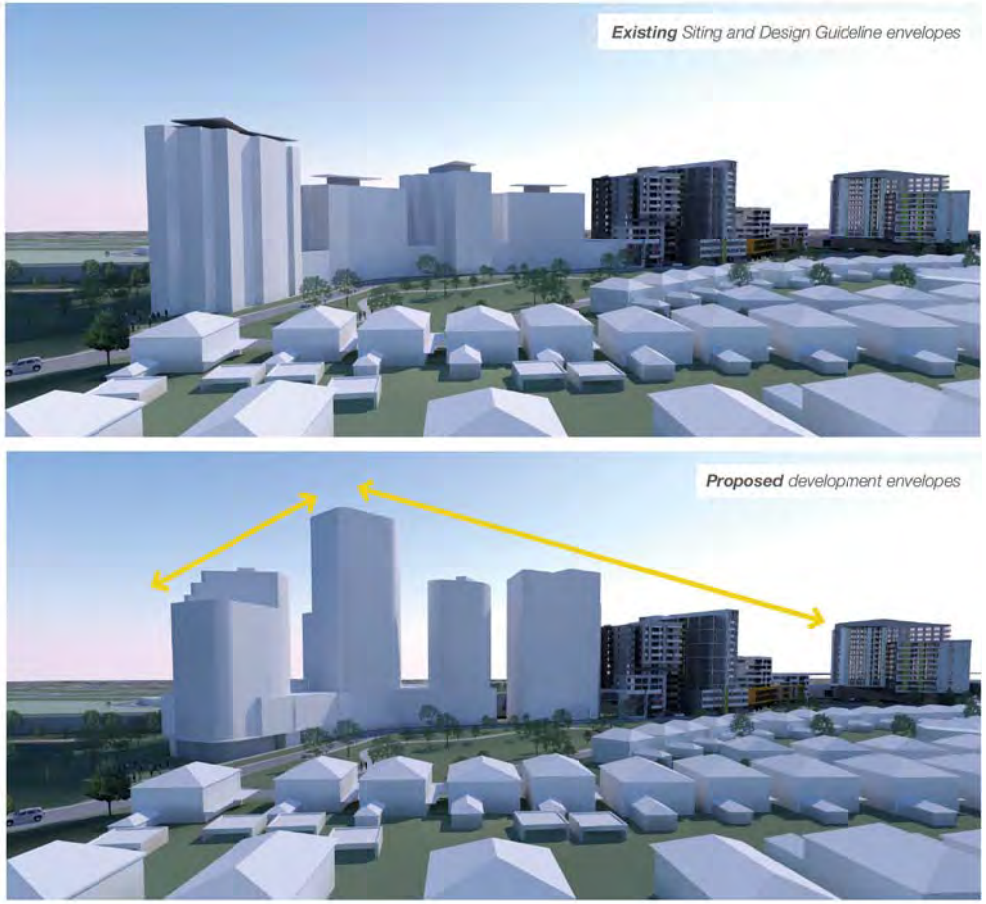
Views towards the city skyline and adjacent park and wetlands are prioritised, while the internal plans avoid apartments close to and facing the freeway directly, due to amenity impacts.

3.2.4 Building spacing, tapering

The arrangement achieves minimum building separation of 10m at the closest points, but the sweeping forms allow the buildings to 'taper' away from each other.



Transitions in built form height and massing will assist views, outlook and access to natural light.

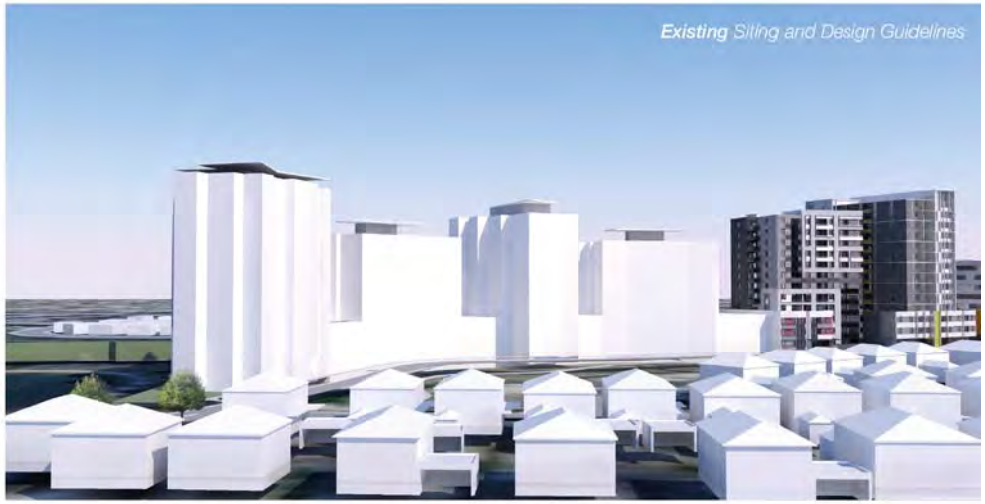


creating wider spacing and enhanced outlook and daylight access for building occupants. This tapering also serves to widen the areas to the east and west from which visibility between buildings is available.

The building footprints also serve to 'open up' space at podium level for communal outdoor spaces and facilities.

3.2.5 Public open space at southern end

The original SDG and Master Plan indicate a compact public open space at the southern 'tip' of the Apartments Precinct, in between the existing public open space



between Galada Avenue and Bunjil Way, and the existing wetlands to the south. This space is indicated as follows:

Document	Proposed 'pocket park'
Master Plan (2004)	479 sq.m (with total open space set at **25,153 sq.m**)
Siting and Design Guidelines (2009)	320 sq.m (scaled off plan – area not specified), with complex shape
Proposed SDG and this UDR (2015)	**430 sq.m** (with total open space delivered at **28,950 sq.m,** including this UDF)

This should be understood in the context that across the Parkville Gardens Freeway Apartments Precinct overall, the area of open space already delivered exceeds that envisaged in the Master Plan, and the total exceedance (including the proposed UDR) will be in the order of 3,797sq.m.

It is envisaged that the proposed open space will be useful for informal meeting, social activity, children's play and sitting/reading, given its favourable orientation to the north / north-east.

There will also be private recreational spaces located on the podium structure of Stages 10/11/12, which will encompass communal activities such as a gymnasium and other recreational facilities.

Increased overall open space

The original Master Plan provided for a total public open space area of 25,153 sq.m across the Parkville Gardens Freeway Apartments Precinct development area. A recent survey demonstrates that this total has been significantly exceeded, and the public open space provided for in this UDR further increases the total open space provision.

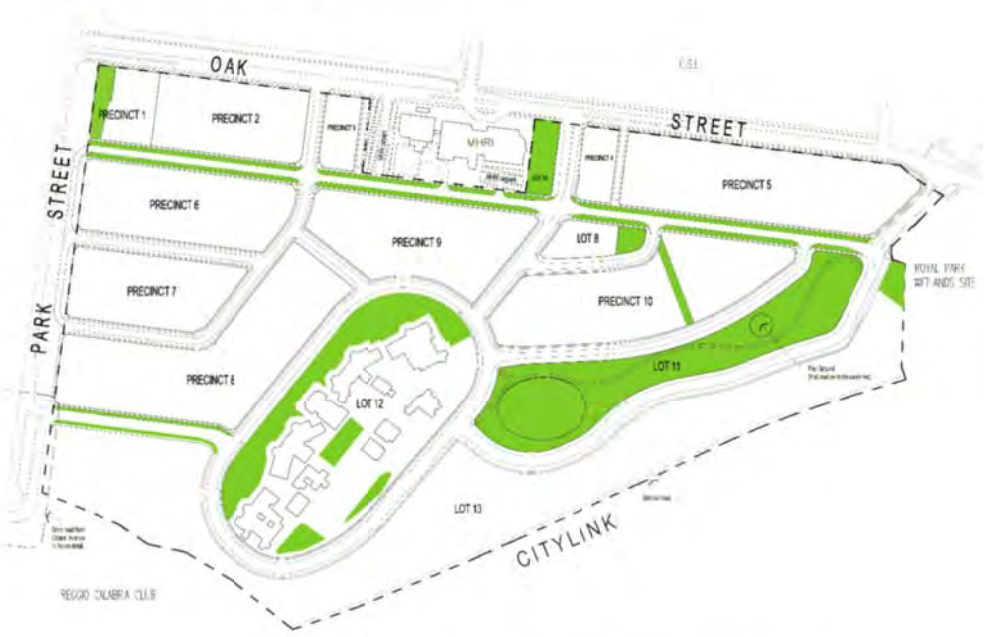
The total delivered open space area is 28,520 sq.m. The additional 430 sq.m (Stage 12 pocket park, outlined above) increases the total open space to 28,950 sq.m. This is 3,797 sq.m more that the Master Plan provided for.

3.2.6 Separation distance from existing houses

The apartment buildings are positioned to maximise separation distances from existing houses within the Parkville Gardens Freeway Apartments Precinct. The

Master Plan (2004), Public Open Space Calculations

Proposed Master Plan (2015), Public Open Space Calculations



Public Open Space

Location	*Master Plan (2004)*		*Proposed Master Plan (2015)*	
Precinct 1	876	m^2	789	m^2
Lot 14	708	m^2	1,158	m^2
Lot 11	11,789	m^2	13,330	m^2
Precinct 10	411	m^2	304	m^2
Lot 8	404	m^2	404	m^2
Lot 12	5,389	m^2	6,796	m^2
		m^2	323	m^2
		m^2	395	m^2
Lot 13	479	m^2	430	m^2
Boulevard Nature Strips	5,097	m^2	5,021	m^2
Total	**25,153**	**m^2**	**28,950**	**m^2**
Additional Public Open Space Provided			**3,797**	**m^2**

highest proposed built form (Stage 11 at 19 habitable storeys) is located at the greatest distance away from existing houses.

Importantly the approximate height of this building (79.5m nominally) is the equivalent of its closest distance to existing houses (68m) as shown. Therefore it reflects a maximum height-to-distance ratio of 1:1, in terms of this formal relationship, which is considered acceptable in this context.

3.2.7 Lower scale, more recessive built form at southern end

The proposed built form framework achieves an important lowering of built form scale at the most sensitive location, being the point closest to existing houses along Bunjil Way.

The proposed framework for Stage 12 incorporates a significant 'step down' in the profile of this building at its eastern point, creating a significantly more recessive form as perceived from nearby residential houses across the park, and from the park and wetlands, resulting in reduced perceived building mass or bulk.

The proposed building height at Stage 12 is 10 habitable levels, but stepping down to 7 habitable storeys as shown in the diagrams below.

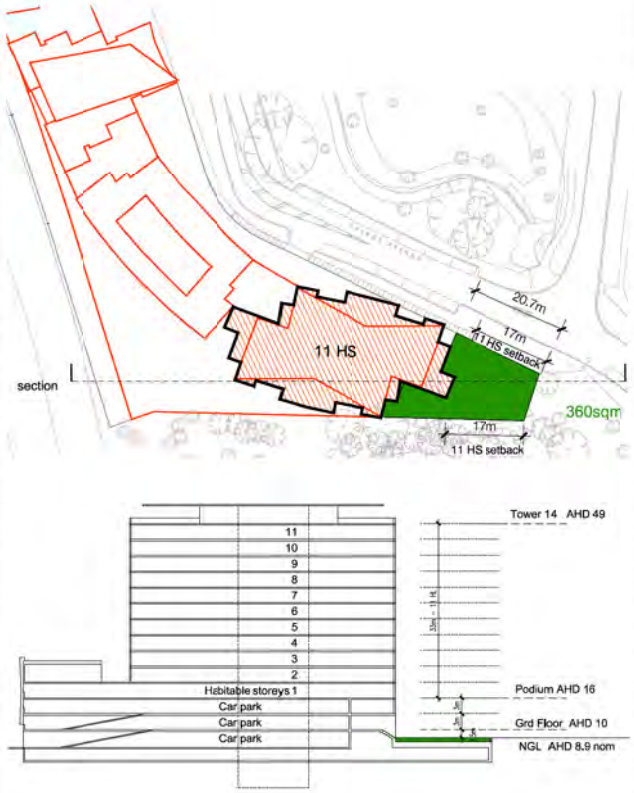
3.2.8 Greater variation and visual interest in built form

The varied building heights provide for increased variation in scale of buildings, and create a 'bookend' or urban 'marker' to define the southern end of the Apartments

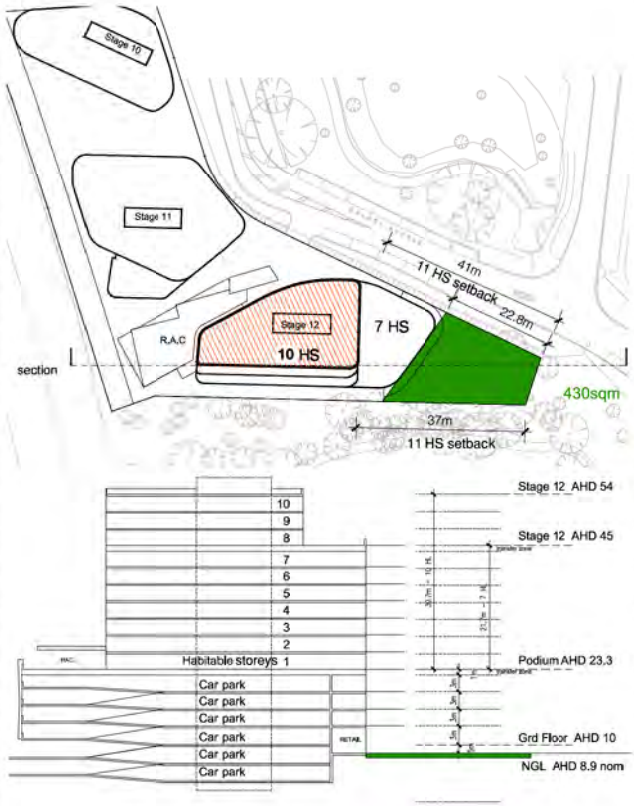
Comparative longitudinal (east-west) cross-sections through Stage 12 building form, showing the more recessive, stepped form now proposed, in response to neighbouring houses.

The 10 habitable levels setback from the edge of the open space has more than doubled from 17m to 37m.

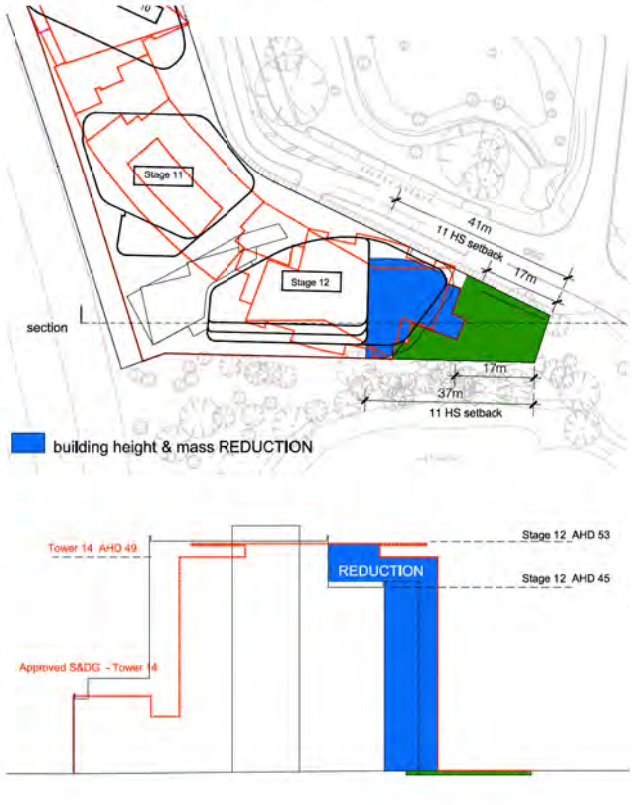
Approved SDG



Proposed SDG



Approved and proposed comparison

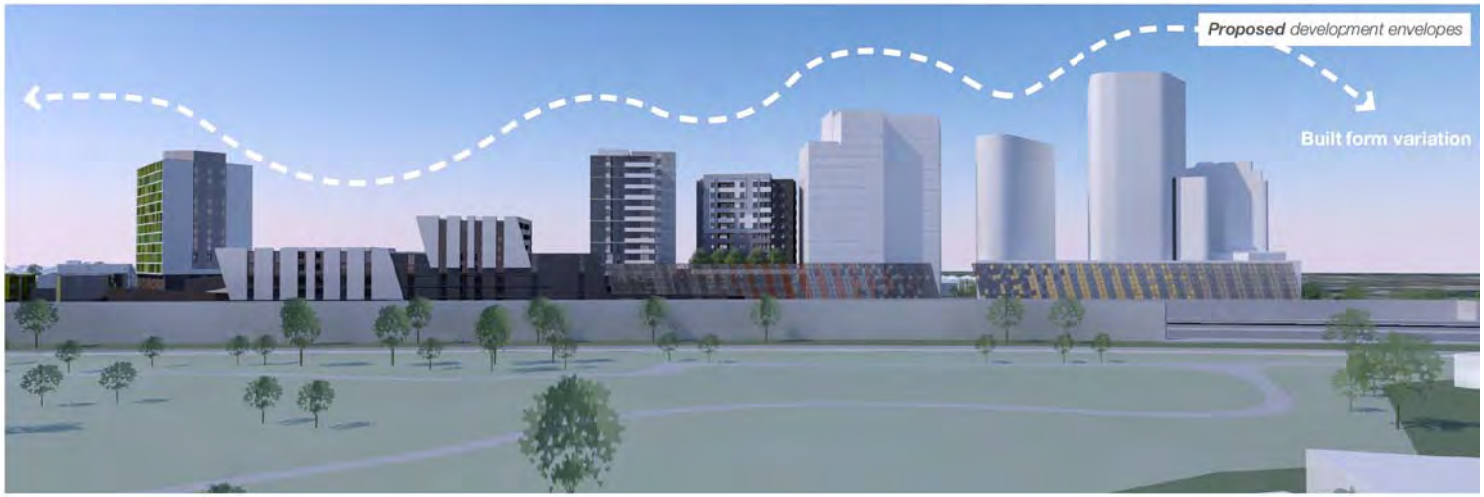


Precinct. The proposed building heights and sequence or transitions (up and down) are as follows (noting some of the buildings incorporate stepped or tapered forms and the numbers below are the upper heights):

- **Stage 9:** 12-15 Habitable Storeys (*going **up** from Stage 8 at 9 Habitable Storeys*), *then **down** slightly to:*
- **Stage 10:** 13 Habitable Storeys, *then **up** substantially to:*
- **Stage 11:** 19 Habitable Storeys, *then **down** substantially to:*
- **Stage 12:** 10 Habitable Storeys, *then stepping **down** by 3 storeys to a lower south component.*

3.2.9 Clear formal relaticnships between the buildings, forming a distinct cluster

Stages 10-12 are identified as a defined and integrated cluster of distinct buildings, which are differentiated from




Existing Siting and Design Guideline envelopes


Existing Siting and Design Guideline envelopes


Proposed development envelopes


Proposed development envelopes

other Stages and from apartment 'typical' building forms. They incorporate irregular, streamlined and sweeping forms in plan, and will be seen as abstract, sculptural objects in the skyline.

Materials and façade details will be selected to identify these three buildings as a 'set piece', and to reinforce perceptions of smooth, fluid and tapering surfaces.

3.3 Built Form Massing

The Stages affected by this UDR incorporate varied building profiles, explained below:

The 'Wolf Point Towers' by Pelli Clarke Pelli

The 'Pebble Towers' by Atkins

Friedrichstrasse Glass Tower (Berlin) by Mark Braun Architects

The Corning Museum of Glass by Birkerts

3.3.1 Stage 9
Rectilinear built form with transition from 12 Habitable Storeys (HS) (to north, in response to the 9HS of the adjacent Stage 8), to 15 HS to the south.

3.3.2 Stage 10
Simple, tapered building form extending to 13 HS, for the full building footprint.

3.3.3 Stage 11
Prominent central building, extending up to 19 HS, with tapering to south down to step in building form at 11HS.

3.3.4 Stage 12
Tapered building form extending to 10 HS, with a significant step down to 7HS to the east, with tiered stepping down to the south.

The proposed building envelopes therefore are profiled and distinct in plan, as well as in elevation or perspective views, creating a complex and integrated cluster of sculptural forms.

3.3 Shadow Assessment

The diagrams below illustrate the comparative shadowing outcomes affecting the existing wetlands to the south of the Apartment Precincts, at the September equinox, for both the current and proposed built form frameworks.

These diagrams demonstrate that, between 11am and 2pm, the proposed framework achieves a slight net reduction in shading impacts to the wetland area.

This is an important outcome, in that it demonstrates the potential for significant modifications to building form in the precinct, while maintaining appropriate levels of off-site impacts, consistent with the current approved envelopes under the SDG.

It also demonstrates that within this parameter, additional built form can be achieved in less-sensitive parts of the site, while reduced building massing in more sensitive parts of the site have a significant benefit for surrounding areas.

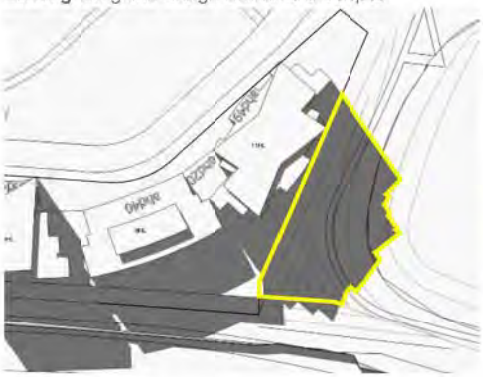
Avoiding increased shadow impacts
The proposed built form response results in a slight net reduction in shadow impacts to the adjacent wetlands area, in comparison to the current *Siting and Design Guidelines* provisions, as demonstrated in the following diagrams.

This outcome was established as an important factor in consideration of changed built form parameters, through discussion with state government officers, and the proposed built form has been 'sculpted' carefully to achieve it.

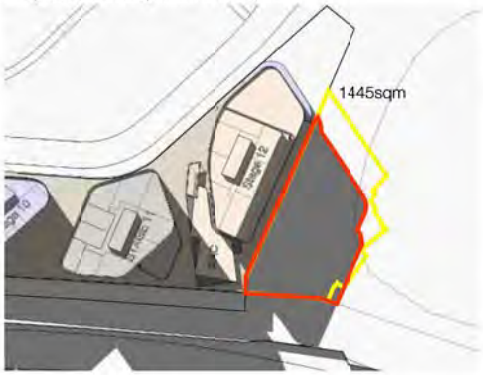
Time	Existing SDG	Proposed	Variation
11am	1,445m²	1,131m²	-314m²
12pm	1,551m²	1,517m²	-34m²
1pm	1,666m²	1,895m²	+229m²
2pm	1,903m²	2,019m²	+116m²
	6,565m²	6,562m²	-3m²

September Equinox, 11am

Existing *Siting and Design Guideline envelopes*

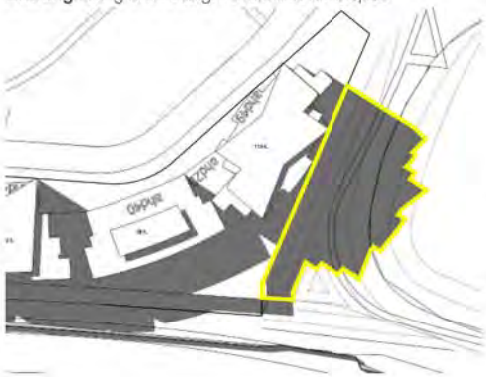


Proposed *development envelopes*

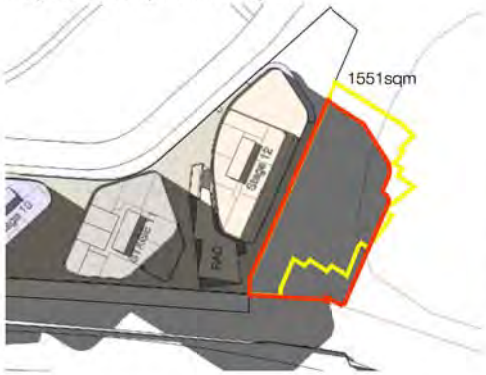


September Equinox, 12pm

Existing *Siting and Design Guideline envelopes*



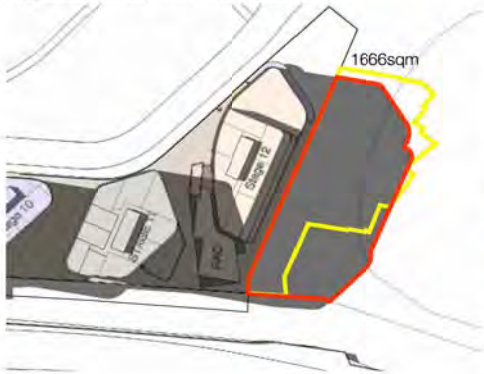
Proposed *development envelopes*



September Equinox, 1pm

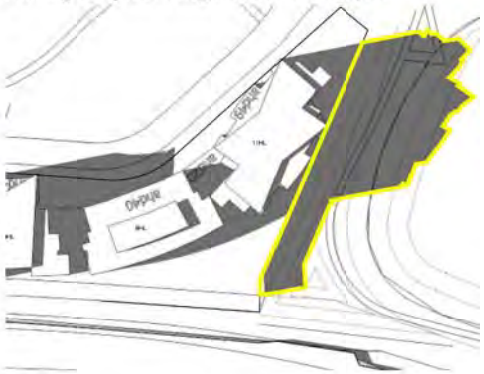
Existing *Siting and Design Guideline envelopes*

Proposed *development envelopes*

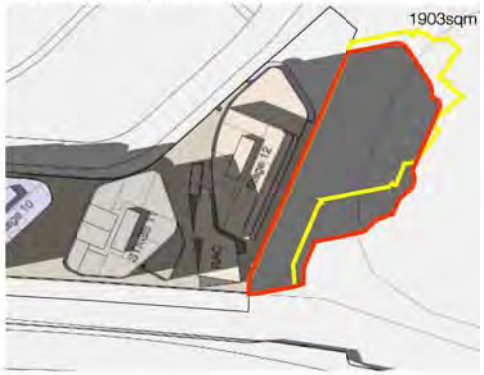


September Equinox, 2pm

Existing *Siting and Design Guideline envelopes*



Proposed *development envelopes*



Existing Siting and Design Guidelines

Proposed development envelopes